



Haringey Council

Please note that the Council has amended the protocol with regard to hearing representations at Planning Sub-Committee meetings. Objectors or supporters should advise the Council by noon on the working day immediately prior to the Sub-Committee meeting (for a Monday meeting this would be by noon on the Friday prior to the Sub-Committee) in order to allow appropriate administrative arrangements to be put in place. The number of speakers will usually be limited to two speaking for a proposal and two speaking against the proposal with a time limit of 3 minutes i.e. a maximum of 6 minutes.

Persons interested in addressing the Committee in relation to an application should contact the Committee Secretariat team on 020 8489 1512 by noon the working day prior to the Planning Committee meeting.

Please be advised that speaking slots will be allocated on a strictly first come first served basis. Discretion will remain with the Chair regarding the number of representations permitted at Planning Committee meetings and time allocated outside of the guideline set out above.



Haringey Council

Please note that whilst the meeting is open to the public, there are no public speaking rights to agenda item 12 as a pre-application briefing

Planning Sub Committee

THURSDAY, 23RD JULY, 2015 at 7.00 pm HRS - CIVIC CENTRE, HIGH ROAD, WOOD GREEN, N22 8LE.

MEMBERS: Councillors Ahmet (Chair), Basu, Beacham, Bevan, Carroll (Vice-Chair), Carter, Doron, Mallett, Patterson, Ryan and Weston

AGENDA

1. **FILMING AT MEETINGS**

Please note this meeting may be filmed or recorded by the Council for live or subsequent broadcast via the Council's internet site or by anyone attending the meeting using any communication method. Although we ask members of the public recording, filming or reporting on the meeting not to include the public seating areas, members of the public attending the meeting should be aware that we cannot guarantee that they will not be filmed or recorded by others attending the meeting. Members of the public participating in the meeting (e.g. making deputations, asking questions, making oral protests) should be aware that they are likely to be filmed, recorded or reported on. By entering the meeting room and using the public seating area, you are consenting to being filmed and to the possible use of those images and sound recordings.

The Chair of the meeting has the discretion to terminate or suspend filming or recording, if in his or her opinion continuation of the filming, recording or reporting would disrupt or prejudice the proceedings, infringe the rights of any individual, or may lead to the breach of a legal obligation by the Council.

2. **APOLOGIES**

3. **URGENT BUSINESS**

The Chair will consider the admission of any late items of urgent business. Late items will be considered under the agenda item where they appear. New items will be dealt with at item 13 below.

4. DECLARATIONS OF INTEREST

A member with a disclosable pecuniary interest or a prejudicial interest in a matter who attends a meeting of the authority at which the matter is considered:

- (i) must disclose the interest at the start of the meeting or when the interest becomes apparent, and
- (ii) may not participate in any discussion or vote on the matter and must withdraw from the meeting room.

A member who discloses at a meeting a disclosable pecuniary interest which is not registered in the Register of Members' Interests or the subject of a pending notification must notify the Monitoring Officer of the interest within 28 days of the disclosure.

Disclosable pecuniary interests, personal interests and prejudicial interests are defined at Paragraphs 5-7 and Appendix A of the Members' Code of Conduct

5. DEPUTATIONS/PETITIONS

To consider receiving deputations and/or petitions in accordance with Part Four, Section B, Paragraph 29 of the Council's Constitution.

6. MINUTES (PAGES 1 - 16)

To confirm and sign the minutes of the Special Planning Sub Committee held on 22 June.

7. PLANNING APPLICATIONS

In accordance with the Sub Committee's protocol for hearing representations; when the recommendation is to grant planning permission, two objectors may be given up to 6 minutes (divided between them) to make representations. Where the recommendation is to refuse planning permission, the applicant and supporters will be allowed to address the Committee. For items considered previously by the Committee and deferred, where the recommendation is to grant permission, one objector may be given up to 3 minutes to make representations.

8. OLD FIRE STATION, TOWN HALL APPROACH ROAD, N15 4RX (PAGES 17 - 42)

Change of use of the ground floor only of the Former Fire Station from an office into a restaurant, including interior alteration and listed building consent for change of use of the ground floor only of the Former Fire Station from an office into a restaurant, including interior alteration.

RECOMMENDATION: Grant planning permission and Listed Building Consent, subject to conditions.

9. STAMFORD HILL PRIMARY SCHOOL, BERKELEY ROAD, N15 6HD (PAGES 43 - 52)

Installation of a new working kitchen to cater for primary school children at Stamford Hill Primary School. Works involve the installation of new cooking appliances and an extract canopy with flue ducting to the outside. (retrospective application).

RECOMMENDATION: Grant permission subject to conditions.

10. BOUNDS GREEN JUNIOR AND INFANT SCHOOL, BOUNDS GREEN ROAD, N11 2QG (PAGES 53 - 76)

Demolition of the existing wing adjacent to Block 4 Key Stage 2 building facing Bounds Green Road and replacement with a two storey extension building including new classroom and teaching rooms. Works also including a new single storey small extension to Block 1 kitchen block to the rear side (northwest) of the site and resurfacing/alterations to external playground.

RECOMMENDATION: Grant permission subject to conditions.

11. PRE-APPLICATION BRIEFINGS

Item 12 is a pre-application presentation to the Planning Sub-Committee and discussion of proposals.

Notwithstanding that this is a formal meeting of the Sub-Committee, no decisions will be taken on this item and any subsequent application will be the subject of a report to a future meeting of the Sub-Committee in accordance with standard procedures.

The provisions of the Localism Act 2011 specifically provide that a councillor should not be regarded as having a closed mind simply because they previously did or said something that, directly or indirectly, indicated what view they might take in relation to any particular matter. Pre-application briefings provide the opportunity for Members to raise queries and identify any concerns about proposals.

The Members' Code of Conduct and the Planning Protocol 2014 continue to apply for pre-application meeting proposals even though Members will not be exercising the statutory function of determining an application. Members should nevertheless ensure that they are not seen to pre-determine or close their mind to any such proposal otherwise they will be precluded from participating in determining the application or leave any decision in which they have subsequently participated open to challenge.

12. TOTTENHAM HOTSPUR STADIUM, BILL NICHOLSON WAY, 748 HIGH ROAD, N17 0AP (PAGES 77 - 88)

13. NEW ITEMS OF URGENT BUSINESS

To consider any items admitted at item 2 above.

14. DATE OF NEXT MEETING

7 September.

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Wednesday, 15 July 2015

**MINUTES OF THE PLANNING SUB COMMITTEE
MONDAY, 22 JUNE 2015**

Councillors: Ahmet (Chair), Basu, Bevan, Carroll (Vice-Chair), Carter, Doron, Hare, Mallett, Patterson and Weston

MINUTE NO.	SUBJECT/DECISION
<p>PC30.</p>	<p>FILMING AT MEETINGS</p> <p>RESOLVED</p> <ul style="list-style-type: none"> • That the Chair's announcement regarding the filming of the meeting for live or subsequent broadcast be noted.
<p>PC31.</p>	<p>APOLOGIES</p> <p>Apologies were received from Cllrs Beacham (for whom Cllr Hare substituted) and Ryan.</p>
<p>PC32.</p>	<p>TOTTENHAM HOTSPUR STADIUM, BILL NICHOLSON WAY, 748 HIGH ROAD N17 0AP</p> <p>The Committee considered a report on the application under s73 of the Town and Country Planning Act 1990 for a minor material amendment to planning permission HGY/2010/1000 granted on 21 September 2011 to provide a new basement level beneath the approved stadium and amendment to the consented ground floor layout. The report set out details of the proposal, the site and surroundings, planning history, relevant planning policy, consultation and responses, analysis, equalities and human rights implications and recommended to grant permission subject to conditions and subject to a s106 legal agreement.</p> <p>The planning officer gave a short presentation highlighting the key aspects of the report.</p> <p>The Committee raised the following points in discussion of the application:</p> <ul style="list-style-type: none"> • An update was sought on the status of the Club's plans for the submission of a further planning application for the stadium site. Officers advised that pre-application discussions were underway on a revised scheme and which would likely come before the Committee as a pre-application briefing in July. • In response to a question regarding the removal of conditions under the s73 application, it was identified that only those already discharged or no longer relevant to the stadium phase would be affected. • The Committee asked that consideration be given under any revised planning application to the inclusion of a condition covering the use of sustainable surface water drainage systems and the inclusion of reference to radio within the current condition requiring investigation of the potential impact of the stadium on TV reception. <p>The Chair moved the recommendation of the report and it was</p> <p>RESOLVED</p>

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- That planning application HGY/2015/0964 be approved subject to conditions and subject to a s106 legal agreement.
1. The 'Stadium' part of the development (see plan A600 Rev 00), shall commence within five years of the date of the original planning permission (HGY/2010/2011) 20.09.2010.
Reason: This condition is imposed by virtue of Section 92 of the Town & Country Planning Act 1990 and to prevent the accumulation of unimplemented planning permissions.
 2. Before any works or development commence on the retained Heritage assets 746,748,750, Tottenham High Road , the following details in respect of each of the retained buildings shall be submitted to and approved in writing by the local planning authority:
 - a. samples of all external materials comprising wall facing materials and cladding, window glass, door and window frames, decorative features, rainwater goods and paving
 - b. elevational drawings, plans, roof plans, and sections at 1:100 scale of all the buildings, sample elevations and sections at 1:20 scale of bays, windows and doors, balconies, parapets and eaves
 - c. drawings at 1:1 scale of sectional profiles of the window and door frames, cornices, string courses and other decorative mouldings
 - d. detailed drawings of any roof mounted plant and equipment and screening measures
 - e. details of the architectural treatment of the northern and eastern elevations of the Old White Hart Public House (No. 750)Reason: To ensure a high quality development to preserve and enhance the character and appearance of the North Tottenham Conservation Area.
 3. The repair and renovation of the retained Heritage assets 746,748,750, Tottenham High Road shall be completed in accordance with a schedule of works contained within the Section 106 Agreement, and to the written satisfaction of the local Planning authority before any major event takes place in the new stadium.
Reason: To ensure a high quality development to preserve and enhance the character and appearance of the North Tottenham Conservation Area.
 4. All approved materials shall be erected in the form of a samples board to be retained on site throughout the works period for the phase concerned and the relevant parts of the works shall not be carried out otherwise than in accordance with the approved details.
Reason: To ensure a comprehensive and sustainable development and to achieve good design throughout the development.
 5. The number of single day major non - association football events per annum (including music concerts) shall be no more than 4 of which no more than 1 shall be on Sunday and no more than 1 on a Bank Holiday (where a major event is a single day event attended by 10,000 visitors or more).
Reason: In order to protect the surrounding residential properties from noise pollution.
 6. No major event shall take place in the stadium outside the hours of 09:00 (09:00am) and 24:00 (12:00am) unless otherwise agreed in writing by the

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Council as Local Planning Authority.

Reason: In order to protect the surrounding residential properties from noise pollution.

7. At least 28 days prior to any amplified music event utilising the auditorium, the owner shall submit for approval by the Local Planning Authority a detailed feasibility study examining the likely propagation of music noise from the proposed event. The study shall have full reference to the guidance of 'The Noise Council's Code of Practice on Environmental Noise Control at Concerts' or subsequent alternative guidance.
Reason: In order to protect the surrounding residential properties from noise pollution.
8. External screens with associated sound system shall not be used inside or outside at the stadium without the prior permission of the Local Planning Authority in writing.
Reason: In order to protect the surrounding residential properties from noise pollution.
9. For the music concert events hereby permitted, amplified sound from concerts within the stadium must be controlled in accordance with guidance provided by The Noise Council's Code of Practice on Environmental Noise Control at Concerts.
Reason: In order to protect the surrounding residential properties from noise pollution.
10. No event involving amplified sound, other than a public address system, shall take place or be operated on the podium or other location outside the stadium itself without the express written permission of the Local Planning Authority.
Reason: In order to protect the surrounding residential properties from noise pollution.
11. Within 2 years of commencing phase 2 of the development, the applicant shall submit a landscape maintenance scheme for each phase of the development which shall be approved in writing by the local planning authority. Any trees or areas of planting which, following completion of the relevant phase of landscaping, die, are removed or become seriously damaged or diseased, shall be replaced as soon as is reasonably possible and, in any case, by not later than the end of the following planting season, with others of similar size and species, unless the local planning authority gives written consent to any variation.
Reason: To ensure a comprehensive and sustainable development, to ensure good design, to ensure that the landscaping is secured in accordance with the Environmental Impact Assessment
12. Unless otherwise agreed in writing by the Local Planning Authority, the car parking provision within the development shall not exceed the following car parking spaces: 401 for the supermarket, 40 for the hotel, 319 for the stadium and 121 for the residential.
Reason: In order to ensure the appropriate levels of car parking in the scheme are not exceeded.

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13. No demolition, construction or building works shall be carried out except between the hours of 0800 and 1800 hours Monday to Friday or before 0800 and 1200 hours on Saturday and not at all on Sundays or bank holidays unless written approval from the Local Planning Authority has been obtained prior to works taking place.
Reason: In order to ensure that the proposal does not prejudice the enjoyment of neighbouring occupiers of their properties in accordance with the Environmental Impact Assessment.
14. Lorries delivering plant or materials during the construction phase of the development will only use designated routes agreed in writing in advance with the Local Planning Authority.
Reason: To minimise the impact of lorry traffic in local residential roads in accordance with the Environmental Impact Assessment.
15. Vehicles may arrive, depart, be loaded or unloaded during the construction phase of the development within the general area of the application site only between 0700 hours and 1800 hours Monday to Friday and 0800 hours and 1200 hours on Saturday and not at all on Sunday or Bank Holidays except with the prior written approval of the Local Planning Authority.
Reason: In order to ensure that the proposed development does not prejudice the free flow of traffic or the conditions of general safety along the neighbouring highway or effect the amenity of local residents in accordance with the Environmental Impact Assessment
16. The surface water drainage details shall include that petrol/oil interceptors shall be fitted in all car parking/washing/repair facilities.
Reason: In order to prevent the pollution of the surface water drainage system in accordance with the Environmental Impact Assessment.
17. At 1 metre outside the windows of any neighbouring habitable rooms the level of noise from plant and machinery shall be at all times at least 5 decibels below the existing background noise levels, expressed in dB (A) at such locations. Where the noise from plant and machinery is tonal in character the differences in these levels shall be at least 10dB (A).
Reason: In order to protect the amenities of the locality in accordance with the Environmental Impact Assessment.
18. Technical specification details of the mechanical plant to be installed within the plant areas shown on the submitted floor plans approved, together with an accompanying acoustic report shall be submitted to and approved by the local planning authority prior to installation of this plant. The plant shall not be operated other than in complete accordance with such measures as may be approved.
Reason: In order to protect the amenities of the locality in accordance with the Environmental Impact Assessment.
19. Unless otherwise agreed in writing by the Local Planning Authority no commercial roof top facilities shall be in use between the hours of 2300 - 0700 hours any day of the week.
Reason: In order to protect the amenities of the locality in accordance with the Environmental Impact Assessment.

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20. Any restaurant (A3), public house and wine bar (A4) or takeaway (A5) uses hereby permitted shall not be operated before 0800 or after 2400 hours on any day of the week.

Reason: In order to ensure that the proposed development does not prejudice the amenities of the future occupiers of the development

21. Prior to construction of the Stadium superstructure further investigations shall be undertaken and approved in writing by the Local Planning Authority of the impact of the construction works on the television reception in the local area:

(a) In particular the 'shadow flicker' on existing and future residential occupants within nearby buildings; and

(b) the impact of operating cranes on local television and radio reception, microwave and other telecommunications links and radar signals.

Reason: To ensure the development can be undertaken without impact on television and/or radio reception to protect residential amenity and in accordance with the assessment and conclusions of the Environmental Impact Assessment.

22. Full details of the location and appearance of the CHP flues, including height, design, location and siting shall be submitted and approved in writing by the Council before work on the CHP commences.

Reason: To ensure a comprehensive and sustainable development and to achieve good design through the development in accordance with the Environmental Impact Assessment.

23. With regard to surface water drainage it is the responsibility of a developer to make proper provision for drainage to ground, water courses or a suitable sewer. In respect of surface water it is recommended that the applicant should ensure that storm flows are attenuated or regulated into the receiving public network through on or off site storage. When it is proposed to connect to a combined public sewer, the site drainage should be separate and combined at the final manhole nearest the boundary. Connections are not permitted for the removal of Ground Water. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required.

Reason: To ensure that the surface water discharge from the site shall not be detrimental to the existing sewerage systems.

24. The development permitted by this planning permission shall only be carried out in accordance with the approved Flood Risk Assessment (FRA). Ref: BDRP0001, Version 6, Final, May 2010 and the following mitigation measures detailed within the FRA:

i. Reducing the surface water runoff from the site by at least 50% for all storm events up to and including the 1 in 100 year critical storm, taking into account the effects of climate change. The peak discharge must not exceed 150l/s/ha.

ii. Provision of storage on site to attenuate all flood events up to and including the 1 in 100 year event, taking into account the effects of climate change.

iii. Identification and provision of safe route(s) into and out of the site to an appropriate safe haven.

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Reason: To prevent flooding by ensuring the satisfactory storage and disposal of the surface water from the site.

25. Prior to the commencement of phase 2 of the development approved by this planning permission (or such other date or stage in development as may be agreed in writing with the Local Planning Authority), a verification plan providing details of the data that will be collected in order to demonstrate that the works set out in the 'Remedial Strategy Phase 2 (Central and Stadium) Area Design Note (07.11.2014) prepared by Buro Happold are complete and identifying any requirements for longer-term monitoring of pollutant linkages, maintenance and arrangements for contingency action shall be submitted to and approved, in writing, by the local planning authority.

Reason: To ensure that the risks to the health and welfare of future occupiers and to the environment are mitigated or eliminated to acceptable standards.

26. If, during development, contamination not previously identified is found to be present at the site then no further development (unless agreed in writing with the Local Planning Authority) shall be carried out until the developer has submitted, and obtained written approval from the Local Planning Authority for, an amendment to the remediation strategy detailing how this unsuspected contamination shall be dealt with.

Reason: To ensure protection of controlled waters.

Please note that the conditions referred to in the minutes are those as originally proposed in the officer's report to the Sub-Committee; any amended wording, additional conditions, deletions or informatives agreed by the Sub-Committee and recorded in the minuted resolution, will, in accordance with the Sub-Committee's decision, be incorporated into the Planning Permission as subsequently issued.

PC33. 161 TOTTENHAM LANE N8 9BU

The Committee considered a report on the application to grant planning permission for the retention and renovation of the existing Tottenham Lane façade and side façade and erection of two additional floors and a building comprising 3x 2 storey houses. The report set out details of the proposal, the site and surroundings, planning history, relevant planning policy, consultation and responses, analysis, equalities and human rights implications and recommended to grant permission subject to conditions and a s106 legal agreement.

The planning officer gave a short presentation highlighting the key aspects of the report. A correction was made to an error contained within the report to the s106 heads of terms where the contribution to the future review of the existing Crouch End Controlled Parking Zone (CPZ) would be £8k and not £7k.

An objector addressed the Committee and raised the following points regarding the application:

- Gratitude was expressed for the Development Management Forum held on the application and the subsequent changes made to the design in light of comments raised by the local community.
- Significant concerns remained outstanding on the scheme and it was felt that the officer report did not reflect the scale and scope of objections received from local residents.

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- The height of the building was of objection, in particular the out of scale fourth floor extension, a viewpoint inline with comments made by the planning inspector on the context of building heights in the area under a 2012 appeal decision on the adjacent KwikFit site.
- A modern addition would be out of keeping with the art deco façade of the existing building.
- The scheme would have a detrimental impact on adjacent Fairfield Road properties, in particular their gardens, from the cramming of 3 houses to the rear of the site up the boundary.
- The Committee were urged to reject the application or at the least add a condition requiring the new brickwork to the art deco building to be the same colour and type as the existing façade.

A representative for the applicant addressed the Committee and raised the following points:

- The scheme would bring back into use an urban brownfield site including vacant, low amenity land to the rear.
- Planning permission had been granted in 2014 for a 6 unit scheme within the existing building envelope and the current application was considered to be an improvement on this scheme.
- The existing art deco façade would be retained, with one additional setback floor visible from the frontage.
- The approved scheme at neighbouring 159 Tottenham Lane would raise building heights in the vicinity and obscure to a large extent the proposed extensions.
- In terms of the impact on Fairfield Road properties, there would be a separation distance of 46m between the rear building line of the main building and the rear windows of the closest Fairfield Road properties. In addition, the mews houses to the rear would be stepped down in nature.
- The scheme would be designated car free.
- All the new units would have private amenity space and had been assessed as causing no harm to adjacent residential properties.

The Committee raised the following points in their discussion of the application:

- In response to a question regarding the noise assessment undertaken, it was advised that background noise levels in the area were high due to the urban location and that the considerable separation distance to the nearest neighbouring properties would help mitigate any noise issues from the scheme.
- Clarification was sought on the reasons for the absence of an affordable housing contribution. Officers reminded the Committee of changes introduced by a recent ministerial statement removing the requirement for affordable housing contributions for schemes with fewer than 10 units. The Council had recently challenged this approach under a planning inspectorate appeal for a separate application but had been unsuccessful. Additionally, the viability assessment submitted by the applicant had been independently assessed and which supported the assertion that an affordable housing contribution would not be viable.
- The access arrangements for the maintenance of the green roofs to the mews houses were questioned. It was advised that the farthest mews house would have a slight setback at first floor level to provide access.
- Concern was raised over the car free nature of the scheme and residents potentially circumventing this by moving cars between the CPZs in the area

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which operated to different hours. Transport officers advised that there was no evidence of this being an issue in the area and that in general it was found that CPZs deterred car ownership within their boundaries.

- Clarification was sought on the local car club arrangement under the s106 heads of terms. It was advised that residents would be provided with free membership to a local car club during the first three years of the scheme, with evidence of this to be provided to the Council.
- It was requested that a restriction be imposed on the installation of external metal security shutters to the ground floor commercial units. Officers advised that the wording of informative 7 could be amended to incorporate this.
- Following a discussion, the Committee felt that the request from the objector for the matching of the new brickwork to the side and rear elevations of the art deco building to the existing was sufficiently covered by condition 3 requiring Council approval for external materials.
- Concerns were raised that not all of the residential units were dual aspect and that limited amenity space would be provided to flats 6 and 8. The applicant advised that although some compromises had been necessary in terms of design, the new scheme was of higher quality than that currently approved and that the recessed balconies to the two flats were considered acceptable for units in an urban area above commercial premises.

The Chair moved the recommendation of the report including the proposed amendment to informative 7 to incorporate a ban on the installation of security shutters and it was

RESOLVED

- That planning application HGY/2014/3139 be approved subject to conditions and subject to a s106 legal agreement.
1. The development hereby authorised must be begun not later than the expiration of three years from the date of this permission, failing which the permission shall be of no effect.
Reason: This condition is imposed by virtue of Section 91 of the Town & Country Planning Act 1990 and to prevent the accumulation of unimplemented planning permissions.
 2. Notwithstanding the information submitted with the application, the development hereby permitted shall only be built in accordance with the following approved plans:
2 (01)00 Rev A, 2 (03)00 Rev A, 2 (03)01 Rev A, 2 (03)02 Rev A, 2 (04)00 Rev A, 2 (04)01 Rev A, 2 (04)02 Rev A, 2 (04)03 Rev A, 2 (05)00 Rev A, 2 (05)01 Rev A, 2 (05)02 Rev A, 2 (12)00 Rev F, 2 (12)01 Rev F, 2 (12)02 Rev F, 2 (12)03 Rev F, 2 (12)04 Rev F, 2 (13)00 Rev C, 2 (13)01 Rev E, 2 (13)02 Rev E, 2 (13)03 Rev D, 2 (14)00 Rev D 2 (14)01 Rev F & 2 (14)02 Rev D
Reason: To avoid doubt and in the interests of good planning.
 3. Samples of all materials to be used in conjunction with the proposed development for all the external surfaces of buildings hereby approved, areas of hard landscaping and boundary walls shall be submitted to, and approved in writing by, the Local Planning Authority before any development is commenced. Samples should include type and shade of cladding, window frames and balcony frames, sample panels or brick types and a roofing material sample

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combined with a schedule of the exact product references. The development shall thereafter be implemented in accordance with the approved samples.

Reason: In order for the Local Planning Authority to retain control over the exact materials to be used for the proposed development and to assess the suitability of the samples submitted in the interests of visual amenity.

4. No development shall take place until full details of soft landscape works have been submitted to and approved in writing by the local planning authority and these works shall be carried out as approved. These details shall include detailed drawings of the planting. The landscaping scheme, once implemented, is to be retained thereafter.

Reason: In order for the Local Planning Authority to assess the acceptability of any landscaping scheme in relation to the site itself, thereby ensuring a satisfactory setting for the proposed development in the interests of the visual amenity of the area consistent with Policy 7.21 of the London Local Plan, Policy SP11 of the Haringey Local Plan 2013 and Policy UD3 of the Haringey Unitary Development Plan 2006.

5. Details of the proposed boundary treatment shall be submitted to and approved by the Local Planning Authority prior to the commencement of the development. The approved boundary treatment shall thereafter be installed prior to occupation of the new residential unit.

Reason: In the interest of the visual amenity of the area and residential amenities of neighbouring occupiers.

6. The details of all levels on the site in relation to the surrounding area be submitted and approved by the Local Planning Authority.

Reason: In order to ensure that any works in conjunction with the permission hereby granted respects the height of adjacent properties through suitable levels on the site.

7. No occupation of the development hereby approved until final details of refuse waste storage and recycling facilities arrangements have been submitted to and approved in writing by the Local Planning Authority. Such a scheme as approved shall be implemented and permanently retained thereafter.

Reason: In order to protect the amenities of the locality and to comply with Policy UD7 'Waste Storage' of the Haringey Unitary Development Plan and Policy 5.17 'Waste Capacity' of the London Plan.

8. Before development commences other than for investigative work:

- (a) A desktop study shall be carried out which shall include the identification of previous uses, potential contaminants that might be expected, given those uses, and other relevant information. Using this information, a diagrammatical representation (Conceptual Model) for the site of all potential contaminant sources, pathways and receptors shall be produced. The desktop study and Conceptual Model shall be submitted to the Local Planning Authority. If the desktop study and Conceptual Model indicate no risk of harm, development shall not commence until approved in writing by the Local Planning Authority.

- (b) If the desktop study and Conceptual Model indicate any risk of harm, a site investigation shall be designed for the site using information obtained from the desktop study and Conceptual Model. This shall be submitted to, and approved in writing by, the Local Planning Authority prior to that investigation being

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carried out on site. The investigation must be comprehensive enough to enable:-

- a risk assessment to be undertaken,
- refinement of the Conceptual Model, and
- the development of a Method Statement detailing the remediation requirements.

The risk assessment and refined Conceptual Model shall be submitted, along with the site investigation report, to the Local Planning Authority for written approval

- (c) If the risk assessment and refined Conceptual Model indicate any risk of harm, a Method Statement detailing the remediation requirements, using the information obtained from the site investigation, and also detailing any post remedial monitoring shall be submitted to, and approved in writing by, the Local Planning Authority prior to that remediation being carried out on site.
9. Where remediation of contamination on the site is required completion of the remediation detailed in the method statement shall be carried out and a report that provides verification that the required works have been carried out, shall be submitted to, and approved in writing by the Local Planning Authority, before the development is occupied.
Reason: To ensure the development can be implemented and occupied with adequate regard for environmental and public safety in accordance with Policy 5.21 of the London Plan and Saved Policy UD3 of the Haringey Unitary Development Plan.
10. The development hereby approved shall achieve Level 4 of the Code for Sustainable Homes. No dwelling shall be occupied until a final Code Certificate has been issued for it certifying that Code Level 4 has been achieved.
Reason: To ensure that the development achieves a high level of sustainability in accordance with Policies 5.1, 5.2, 5.3 and 5.15 of the London Plan and Policies SP0 and SP4 the Haringey Local Plan 2013.
11. No development shall take place, including any works of demolition, until a Construction Management Plan, to include details of:
- a. Measures for propping/ safeguarding the façade during construction;
 - b. loading and unloading of plant and materials;
 - c. storage of plant and materials;
 - d. programme of works (including measures for traffic management);
 - e. provision of boundary hoarding behind any visibility zones;
 - f. wheel washing facilities.
- have been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented and retained during the part demolition and construction period.
Reason: To ensure there are no adverse impacts on the free flow of traffic on local roads and to safeguard the amenities of the area consistent with Policies 6.3, 6.11 and 7.15 of the London Plan Policies SP0 of the Haringey Local Plan 2013 and Saved Policy UD3 of the Haringey Unitary Development Plan 2006.
12. a) Any proposal relating to the installation of possible noise creating equipment (i.e. air conditioning plant, mechanical ventilation equipment) shall first be submitted to and approved in writing by the Local Planning Authority prior to any development taking place.

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b) Noise resulting from the use of any external plant, machinery or equipment (or any which is audible outside the site) shall not exceed a level of 5dB(A) below the existing lowest background noise level during the time of operation (or 10dB(A) below if there is a particular tonal quality), when measured according to British Standard BS4142-1997.

Reason: In order to ensure that the proposed development does not prejudice the enjoyment by neighbouring occupiers of their properties

13. The proposed development shall have a central dish/aerial system for receiving all broadcasts for all the residential units created, details of such a scheme shall be submitted to and approved by the Local Planning Authority prior to the occupation of the property and the approved scheme shall be implemented and permanently retained thereafter.

Reason: In order to protect the visual amenities of the neighbourhood.

14. No development shall commence until details of a scheme for the green roofs for the development hereby permitted have been submitted to and approved in writing by the Local Planning Authority. The details shall include its (their) type, vegetation, location and maintenance schedule. The development shall be implemented in accordance with the approved scheme prior to its first occupation and the vegetated or green roof shall be retained thereafter. No alterations to the approved scheme shall be permitted without the prior written consent of the Local Planning Authority.

Reason: To ensure a sustainable development consistent with Policy 5.11 of the London Plan and Policies SP0, SP4 and SP11 of the Haringey Local Plan 2013.

15. No development shall take place until details of a scheme for installing external lighting within the site, including night-time security lighting and its means of actuation, light spread and average illuminance, have been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out entirely in accordance with the approved details.

Reason: In order to retain control over the external appearance of the development and in the interest of the visual amenity of the area.

16. No occupation of the flats hereby approved shall be occupied until the cycle facilities serving it have been provided in accordance with the approved details, and they shall thereafter be retained for their intended purpose unless otherwise agreed in writing by the local planning authority.

Reason: To ensure the development provides adequate cycle parking facilities in accordance with the London Plan.

INFORMATIVE 1: -- Thames Water

Thames Water will aim to provide customers with a minimum pressure of 10m head (approx 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.

INFORMATIVE 2: -- Asbestos Survey

Prior to refurbishment of existing buildings, an asbestos survey should be carried out to identify the location and type of asbestos containing materials. Any asbestos containing materials must be removed and disposed of in accordance with the

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correct procedure prior to any demolition or construction works carried out.

INFORMATIVE 3: - Hours of Construction Work

The applicant is advised that under the Control of Pollution Act 1974, construction work which will be audible at the site boundary will be restricted to the following hours:- 8.00am - 6.00pm Monday to Friday 8.00am - 1.00pm Saturday and not at all on Sundays and Bank Holidays.

INFORMATIVE 4: - Community Infrastructure Levy

The application is advised that the proposed development will be liable for the Mayor of London's CIL. Based on the information given in the plans, the Mayor's CIL charge will be £11,795 (337 x £35) and Haringey CIL charge will be £89,305 (337 x 265). This will be collected by Haringey after the scheme is implemented and could be subject to surcharges for failure to assume liability, for failure to submit a commencement notice and/or for late payment, and subject to indexation in line with the construction costs index. An informative will be attached advising the applicant of this charge.

INFORMATIVE 5: The new development will require numbering.

The applicant should contact the Local Land Charges at least six weeks before the development is occupied (tel. 020 8489 5573) to arrange for the allocation of a suitable address.

INFORMATIVE 6: The applicant's attention is drawn to the Party Wall Act 1996 which sets out requirements for notice to be given to relevant adjoining owners of intended works on a shared wall, on a boundary or if excavations are to be carried out near a neighbouring building.

INFORMATIVE 7: A separate application will be required for either the installation of a new shopfront or the display of any illuminated signs.

INFORMATIVE 8: The applicant is advised that this planning permission does not convey the right to enter onto or build on land not within his ownership.

INFORMATIVE 9: The proposed pruning works to the Leyland cypress in the garden/ ownership of an adjoining property must be carried out sensitively taking into consideration natural target pruning techniques with the works carried out under the supervision of the arboricultural consultant.

PC34. FORMER ST ANN'S POLICE STATION, 289 ST ANNS ROAD, N15 5RD

The Committee considered a report on the application to grant planning permission for the demolition of extensions and outbuildings and conversion of the former St Ann's Police Station to erect a new residential building to provide 32 dwelling units and four bedroom houses. The report set out details of the proposal, the site and surroundings, planning history, relevant planning policy, consultation and responses, analysis, equalities and human rights implications and recommended to grant permission subject to conditions and subject to a s106 legal agreement.

The planning officer gave a short presentation highlighting the key aspects of the report. The Committee were read out a letter of objection received from David Lammy MP concerned with the low proposed affordable housing contribution and

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an email from local ward councillors B. Blake and Morton expressing broad support for the scheme.

An objector addressed the Committee and raised the following points:

- Demolition should not be permitted of the rear extension to the police station.
- The scale and massing of the new building would create a street canyon effect for pollution.
- Additional concerns with the scheme were raised over parking provision, site cramming and the low affordable housing contribution.
- The accuracy of the PTAL rating (public transport accessibility level) awarded to the site was questioned, including the disparity with the rating given to the nearby St Ann's hospital site and errors given in the details of bus routes serving the area.
- Concerns were raised about the proposed height of the new building, particularly as it would overlook a children's playground.
- A lack of detail had been provided on waste management arrangements for the site.

A representative for the applicant addressed the Committee and raised the following points:

- The applicant, One Housing, was a non profit housing organisation.
- The scheme would bring a heritage asset back into use including securing ongoing maintenance as well as providing a good mix of new housing units.
- The applicant had consulted with local residents on the scheme and taken concerns raised into account.
- The scheme was of high design quality.
- The physical constraints of the conversion of the locally listed police station building impacted on the cost of the scheme and thereby the viability assessment.
- Onsite parking spaces would be allocated to the larger family and disabled access units.

The Committee raised the following points in discussion of the application:

- Although a copy of the viability assessment had been circulated to members of the Committee under confidential copy, concern remained on the low 13% affordable housing contribution proposed. The applicant advised that this figure reflected the maximum possible taking into account the costs associated with the conversion of the locally listed building. Additional benefits associated with the scheme were emphasised including s106 contributions, the provision of new housing and local employment opportunities.

Further assurances were sought from officers regarding the contribution put forward by the applicant. Officers informed that the viability assessment had been independently assessed and which had validated the 13% level as a consequence of the sales values of the area and refurbishment costs. A mechanism would be added to any permission to review the contribution should the scheme not be implemented within 18 months. The legal officer reminded the Committee that as the affordable housing contribution had been assessed as policy compliant, legally it could not successfully be used as a grounds for refusal.

- Concerns were raised over the potential for the police station building to be visually diminished from the variation in finish selected for the new building. Officers advised that the approach of varying the materials used and window

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design to the new building had been taken to avoid a pastiche.

- Clarification was sought on whether the density of the scheme had been reduced following the Committee's consideration of the scheme at pre-application stage. It was advised that a slight reduction had been made and that officers were satisfied that the density was within acceptable range taking into account the PTAL of the site.
- The potential of adding a condition to require the planting of additional trees to the Hermitage Road street frontage was queried. The applicant agreed to discuss with officers the potential incorporation of this within the landscaping condition.
- Concerns were raised over the Conservation Officer's initial objections to the scheme and that the officer was not present at the meeting to outline whether these had now been allayed. The Head of Development Management apologised for the officer not being present but confirmed that her initial concerns regarding the proposed variation in colour of bricks to the new building had been allayed through her input into the discharge of condition 3 covering the approval of external materials. The wider overall benefit of bringing the locally listed building back into use had also been taken into account.
- Clarification was sought on the number of single aspect units within the scheme. The applicant advised that there would be two sited in the new building as a consequence of the orientation of the police station building.
- Concerns were raised over the potential storage of refuse bins on the pavement. It was advised by the applicant that well designed, on-street bin stores were planned as part of an effective waste management plan for the scheme.
- The allocation arrangements for the onsite parking spaces were queried. The applicant advised that although allocation principles had yet to be decided, priority would likely to given to the larger family units. In response to concerns regarding the impact of the scheme on on-street parking provision, it was identified that the s106 agreement would include a contribution towards car club membership and a future CPZ review.
- Clarification was sought on the concerns raised by the NHS Mental Health Trust of the scheme causing overlooking to mental health accommodation. Officers advised that they had taken the view that the separation distances and angle of window to window sightlines would be sufficient to safeguard privacy in this regard.

Cllr Bevan put forward a motion, seconded by Cllr Carter, to reject the application on the grounds of overdevelopment as identified by the Design Review Panel, the outstanding concerns of the Conservation Officer regarding the impact of the design of the scheme on the Conservation Area and the allocation by private sale of the parking spaces and the lack of their allocation to the affordable housing units. At a vote, the motion was carried. It was therefore

RESOLVED

- That planning application HGY/2015/0034 be refused on the grounds of overdevelopment as identified by the Design Review Panel, the outstanding concerns of the Conservation Officer regarding the impact of the design of the scheme on the Conservation Area and the allocation by private sale of the parking spaces and their lack of allocation to the affordable housing units.

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PC35.	DATE OF NEXT MEETING 6 July.
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COUNCILLOR AHMET

Chair

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Planning Sub Committee 23 July 2015

Item No.

REPORT FOR CONSIDERATION AT PLANNING SUB-COMMITTEE

1. APPLICATION DETAILS	
Reference No: HGY/2015/1405 & HGY/2015/1406	Ward: Tottenham Green
Address: Old Firestation, Town Hall Approach Road, London, N15 4RX	
Proposal 1: Change of use of the ground floor only of the Former Fire Station from an office into a restaurant, including interior alteration	
Proposal 2: Listed building consent for change of use of the ground floor only of the Former Fire Station from an office into a restaurant, including interior alteration	
Applicant: Create London	
Ownership: Private	
Case Officer Contact: Gareth Prosser	
Site Visit Date: 16.06.15	
Date received: 26/05/2015	Last amended date: N/A
Drawing number of plans: 01-100 Location Plan 1:1250 A3 * A 01-101 Site Plan As Existing 1:200 A1 * A 01-111 Ground Floor Plan As Existing 1:100 A1 * A 01-112 Public Realm Plan As Existing 1:100 A1 * A 01-121 Section AA As Existing 1:50 A1 * A 01-122 Section BB As Existing 1:50 A1 * A 01-131 Elevation AA As Existing 1:100 A1 * A 01-132 Elevation BB As Existing 1:100 A1 * A 01-133 Elevation CC As Existing 1:100 A1 * A 01-141 Site Elevation CC As Existing 1:200 A1 * A 01-201 Site Plan As Proposed 1:200 A1 * A 01-211 Ground Floor As Proposed 1:100 A1 * A 01-212 Public Realm As Proposed 1:100 A1 * A 01-301 Section AA As Proposed 1:50 A1 * A 01-302 Section BB As Proposed 1:50 A1 * A 01-401 Elevation AA As Proposed 1:100 A1 * A 01-402 Elevation BB As Proposed 1:100 A1 * A 01-403 Elevation CC As Proposed 1:100 A1 * A 01-404	

SUMMARY OF KEY REASONS FOR RECOMMENDATION

- This proposal to provide innovative healthy eating is welcomed and supported in principle subject to the detailed consideration of material planning considerations.
- The proposed scheme as assessed by the Council's Conservation Officer is acceptable, both preserving and enhancing the appearance of the Grade II listed building as well as other heritage assets within the immediate vicinity and the wider conservation area. The application is therefore in accordance with The National Planning Policy Framework (NPPF), Policy 7.8 'Heritage Assets and Archaeology' of the London Plan, Haringey Local Plan Policy SP12 and saved UDP policies CSV4 'Conservation', TCR5 'A3 restaurants and cafes, A4 drinking Establishments and A5 hot food takeaways' and UD3 'General Principles'
- The proposed change of use is acceptable and does not have a significant, detrimental impact on the amenity of the neighbouring properties. Conditions have been applied regarding noise and opening hours.

RECOMMENDATION

That the Committee resolve to GRANT planning permission and that the Head of Development Management is delegated authority to issue the planning permission and impose conditions and informatives to secure the following matters:

Proposal 1: Conditions (Full Planning)

- 1) Development begun no later than three years from date of decision;
- 2) In accordance with approved plans and reports submitted;
- 3) Opening Hours
- 4) Noise
- 5) Materials and repairs
- 6) Hidden Features
- 7) Details of proposed fixtures

Proposal 2: Conditions (Listed Building Consent)

- 1) Development begun no later than three years from date of decision;
- 2) In accordance with approved plans and reports submitted;

Informatives:

Environmental Health Requirements

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Appendix 1: Consultation Responses

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3.0 PROPOSED DEVELOPMENT AND LOCATION DETAILS

Proposed development

- 3.1 A planning application has been submitted for planning and listed building consent for a change of use to the ground floor of the Old Fire Station, Tottenham from offices (B1 Use Class) to a restaurant and cafe use (A3 Use Class), with some internal alteration which includes restoring the original height of the ground floor ceiling. The concept is for an innovative healthy eating alternative to the traditional chicken shop.

Background

- 3.2 The Chicken Town proposal was awarded Opportunity Investment Funding (OIF) in January 2015 to secure the essential capital and start-up costs associated with locating and establishing the project for the long-term in Tottenham. Following the start-up phase of the project, it is designed to be revenue generating and self-financing, with all money generated going towards sustaining the project for the long term and providing benefits to the local area. The funding package through OIF includes £210,000 as loan repayable over 8 years and £90,000 as a grant payment. A further £55,000 was raised by the social enterprise through an online 'Kickstarter' campaign. A number of outputs (including initial job creation of 10 full time and 6 part time staff plus 4 apprenticeship positions) have been set out through the grant and loan agreements between the Council and Chicken Town to maximise the economic benefits of the proposal for local residents and Tottenham.

Site and Surroundings

- 3.3 The site is located in the Former Fire Station on Town Hall Approach Road in Tottenham. Also occupying the building are the London Youth Support Trust whose offices are above the proposed restaurant.
- 3.4 The Former Fire Station is one of a trio of related buildings which were built together on Tottenham Green at the turn of the 20th Century. The other buildings are Tottenham Town Hall, which is a venue, and the Bernie Grant Arts Centre, comprising an auditorium, gallery, cafe, and business units, which occupies the site of the old Public Baths building the front of which has been retained. These three buildings face onto Tottenham Green, which is both a historically significant common, as well as a key open space on the long Tottenham High Road.
- 3.5 To the south and the north of the building are two buildings belonging to the College of Haringey, Enfield and North London: a further education college. To the rear of Tottenham Town Hall the Newlon Housing Trust has recently completed Isobel Place, 109 apartments for social rent or shared ownership. There is a proposed access gate to this development between the Fire Station and the Town Hall which does not form part of this application.

- 3.6 On the other side of the Green is Tottenham High Road which is a thriving high street full of shops, restaurants, supermarkets and cafés that runs from Stamford Hill to the North Circular.

Relevant Planning and Enforcement history

- 3.7 HGY/1997/0497 GTD 29-07-97 Tottenham Green Fire Station Town Hall Approach Road London Listed Building Consent for alterations involving the cleaning windows to rear dormer. and repair of principal elevations, roof repairs and casemen.
- HGY/1998/0256 GTD 28-07-98 Former Fire Station & Fireman's Cottages Town Hall Approach Road London Erection of three storey office block including redecoration and minor alterations and repairs to existing cottages for use as starter units.
- HGY/1998/0257 GTD 28-07-98 Former Fire Station & Fireman's Cottages Town Hall Approach Road London Listed Building Consent for the formation of three window openings, temporary demolition of rear part of first floor, rebuilding of first floor area, including walls, windows and roof, at higher level, new windows to existing fire station.
- HGY/2001/1203 GTD 09-10-01 Former Fire Station Town Hall Approach Road London Creation of access gate in front brickwall to provide pedestrian access and alterations to access road kerb.
- HGY/2001/1221 GTD 09-10-01 Former Fire Station Town Hall Approach Road London Listed Building Consent for alteration/opening in existing wall, providing new gate and associated road kerb modification.

4. CONSULTATION RESPONSE

- 4.1 The following statutory bodies, internal consultees and local groups were consulted on this application:

LBH Transportation
LBH Cleansing East
LBH Food and Hygiene
LBH Licensing
LBH Noise and Pollution
LBH Conservation
Tottenham CAAC
Local Residents

- 4.2 The following responses were received:

Internal:

- 1) Conservation Officer – No Objection, subject to conditions.

- 2) Commercial Environmental Health – No objection, however details of the required ventilation and extraction ducting has been supplied.
- 3) LBH Transportation - No objection

External:

- 1) Thames Water – No objection
- 2) Tottenham CAAC – concerns about the impact the proposal may have on the listed buildings and conservation area.

5. LOCAL REPRESENTATIONS

- 5.1 The residents of 175 properties were consulted on the application and site notices were displayed in the vicinity of the site. The number of representations received from residents in response to notification and publicity on this application are as follows:

Objecting: 31
Supporting: 2
Others: 1

- 5.2 The following issues were raised in representations and are material to the determination of the application and are addressed in the next section of this report:

1. Detrimental Impact of character and appearance of the listed building and conservation area
2. Noise and odours
3. Impact to deliveries/HGV's on neighbouring businesses and properties
4. Oversupply of 'chicken restaurants'
5. Inappropriate location
6. Increased parking pressure
7. Clarification is required regarding the proposal being a restaurant or a takeaway
8. Opening times too long
9. Street furniture would encourage loitering and disturbance to existing business.
10. Overlooking of neighbouring properties
11. Inappropriate signage
12. A healthy food initiative would be a 'progressive step for Tottenham'
13. The benefits of a sustainable restaurant use outweigh the loss of any aesthetic appeal

- 5.2 The following issues were raised in representations and are not material to the determination of the application.

1. Anti-social behaviour/litter

6. MATERIAL PLANNING CONSIDERATIONS

6.1 The main planning issues raised by the proposed development are:

1. Principle of development
2. Impact on the Character and Appearance of the Listed building and the Conservation Area
3. Impact on Amenity of Neighbouring properties/businesses
4. Change of Use to A3 Restaurant
5. Parking

Principle of development

6.2 Local Plan policy SP14 sets out that the Council will seek to improve health and wellbeing in Haringey. This proposal to provide innovative healthy eating is welcomed and supported in principle subject to the planning considerations set out below.

Impact on the Character and Appearance of the Listed building and the Conservation Area

6.3 The Legal Position on impacts on heritage assets is as follows, and Section 72(1) of the Listed Buildings Act 1990 provides:

"In the exercise, with respect to any buildings or other land in a conservation area, of any functions under or by virtue of any of the provisions mentioned in subsection (2), special attention shall be paid to the desirability of preserving or enhancing the character or appearance of that area." Among the provisions referred to in subsection (2) are "the planning Acts".

6.4 The Barnwell Manor Wind Farm Energy Limited v East Northamptonshire District Council case tells us that "Parliament in enacting section 66(1) did intend that the desirability of preserving the settings of listed buildings should not simply be given careful consideration by the decision-maker for the purpose of deciding whether there would be some harm, but should be given "considerable importance and weight" when the decision-maker carries out the balancing exercise."

6.5 The Queen (on the application of The Forge Field Society) v Sevenoaks District Council says that the duties in Sections 66 and 72 of the Listed Buildings Act do not allow a Local Planning Authority to treat the desirability of preserving the settings of listed buildings and the character and appearance of conservation areas as mere material considerations to which it can simply attach such weight as it sees fit. If there was any doubt about this before the decision in Barnwell, it has now been firmly dispelled. When an authority finds that a

proposed development would harm the setting of a listed building or the character or appearance of a conservation area, it must give that harm considerable importance and weight. This does not mean that an authority's assessment of likely harm to the setting of a listed building or to a conservation area is other than a matter for its own planning judgment. It does not mean that the weight the authority should give to harm which it considers would be limited or less than substantial must be the same as the weight it might give to harm which would be substantial. But it is to recognise, as the Court of Appeal emphasized in *Barnwell*, that a finding of harm to the setting of a listed building or to a conservation area gives rise to a strong presumption against planning permission being granted. The presumption is a statutory one, but it is not irrebuttable. It can be outweighed by material considerations powerful enough to do so. An authority can only properly strike the balance between harm to a heritage asset on the one hand and planning benefits on the other if it is conscious of the statutory presumption in favour of preservation and if it demonstrably applies that presumption to the proposal it is considering.

- 6.6 In short, there is a requirement that the impact of the proposal on the heritage assets be very carefully considered, that is to say that any harm or benefit to each element needs to be assessed individually in order to assess and come to a conclusion on the overall heritage position. If the overall heritage assessment concludes that the proposal is harmful then that should be given "considerable importance and weight" in the final balancing exercise having regard to other material considerations which would need to carry greater weight in order to prevail.
- 6.7 Policy 7.8 'Heritage Assets and Archaeology' of the London Plan requires development to conserve the significance of the heritage asset. Haringey Local Plan Policy SP12 seeks to ensure the conservation of the historic significance of Haringey's heritage assets, their setting, and the wider historic environment. Policy CSV4 states that it is required that alterations and extensions to listed buildings to:
- be necessary and not detrimental to the architectural and historical integrity and detailing of a listed building's interior and exterior;
 - relate sensitively to the original building; and not adversely affect the setting of a listed building.
- 6.8 The impact of the proposed change of use including external alterations to the front facade and setting of the Grade II listed building must be considered within the wider context of the Municipal buildings fronting Tottenham Green, the wider conservation area and the existing public realm.

- 6.9 In order to convert the building, the scheme proposes to remove the existing partitions and suspended ceilings. These currently detract from the building and their removal would allow the appreciation of the entire volume of the space, a significant feature pertaining to the original architecture and function of the building. As such the internal works would positively enhance the significance of the building and would be acceptable. Other works required for the operation of the restaurant would not have any impact on the fabric of the listed building and would be acceptable.
- 6.10 The new glazed entrance would replace a modern intervention and would have no impact on the fabric of the listed building. The extract required for the operations of the new use is proposed to be installed on the southern elevation. This elevation is of very little significance architecturally and already has a myriad of later fixtures including an existing vent pipe. As such the proposed duct would cause no additional impact to the fabric of the building and would be acceptable.
- 6.11 The scheme also proposes works to the immediate paved area around it. Existing car parking is proposed to be removed. There would be tree planters, some up lighters and signage along with a gold anodised steel post with a small sculpture. The works would have no impact on the fabric of the listed building but would have some impact on its setting. Given the condition and nature of the existing public realm, which is neglected and in need of renewal, the proposed works are considered to enhance the building's setting as well as the Town Hall and the wider conservation area by improving the treatment of the existing flower bed with the use of reconstituted stone edging and the inclusion of up lighters to highlight the architectural features of the fire station. Painting of the existing wall behind the flower beds is also considered to be a simple but effective improvement to the setting.
- 6.12 Overall, the proposed scheme as assessed by the Council's Conservation Officer would be acceptable, both preserving and enhancing the appearance of the Grade II listed building as well as other heritage assets within the immediate vicinity and the wider conservation area and not causing harm. The application is therefore in accordance with The National Planning Policy Framework (NPPF), Policy 7.8 'Heritage Assets and Archaeology' of the London Plan, Haringey Local Plan Policy SP12 and saved UDP policy CSV4.

Impact on Amenity of Neighbouring properties/businesses

- 6.13 UD3 General Principles requires development proposals to demonstrate that there is no adverse impact on residential amenity or other surrounding uses in terms of loss of daylight or sunlight, privacy overlooking and aspect etc. UD3 also requires proposals to complement the character of the local area.
- 6.14 Overall 31 objection letters have been received, with two letters of support also received. The objections can be split into two broad categories, those from residents of Isobel Place which is located at the back of Town Hall Approach and from LYST tenants within the Fire Station building. Objections from Isobel Place primarily relate to the impact of the restaurant on the immediate

environment and anti-social behaviour issues that might arise. Clarification has also been sought regarding the property being used as a takeaway.

- 6.15 The application is for a restaurant use (A3), not a hot food takeaway (A5), so the nature of its operation will be different to takeaways that are more typically associated with anti-social behaviour. The restaurant concept is designed to appeal to a mix of different groups of people and this diverse customer base will reduce the likelihood of anti-social behaviour being a problem. In addition there is no solid evidence to support concerns relating to such behaviour. All applications for restaurant uses are subject to conditions which the operators must adhere to, regarding ventilation, waste and fumes, to protect local resident and business amenity (see conditions and informatives).
- 6.16 Concerns have been raised regarding patrons overspilling from the outdoor seating area into Isobel Place. Isobel Place is set well back (approximately 85m) from the rear of the old fire station and therefore the likelihood of additional pedestrian traffic along the route is unlikely. In addition, Newlon Housing which manages Isobel Place has been granted planning permission to install a vehicular and pedestrian gate at the entrance to the development, which would significantly reduce any potential impact for residents. Newlon Housing has confirmed that it intends to implement this planning permission this year and as early as is possible.
- 6.17 Several objections have also been received from business tenants currently located within the Fire Station building. In particular, tenants have voiced concerns regarding operational matters, should the planning application be approved. The main function of the current reception area at the front of the Old Fire Station is to manage deliveries to the tenants and also as a front face for LYST (London Youth Support Trust) and the TGEC (Tottenham Green Enterprise Centre). The continuity of these two key functions and alternative areas for managing deliveries and having a point of contact for tenants are being considered by all stakeholders involved (LYST, Chicken Town, CONEL and the Council).
- 6.18 Financial and technical support has been offered to LYST by Create (the arts organisation which is the commissioning body for Chicken Town) on these matters, including designing upgraded signage for the front of the building. Among the options being explored is maintaining the existing reception use on site within the first floor of the existing building.
- 6.19 Whilst the concerns expressed above are noted, the planning department is satisfied that alternative arrangements such as those proposed above can overcome these concerns and a successful reception space can operate at an alternative location. The proposed relocation of the reception to the first floor of the fire station would be supported should this option be put forward.
- 6.20 Proposed changes to the public realm outside the proposed restaurant are modest, including an improved edge to the existing tree/flower bed as well as cycle parking, up lighting and signage. Whilst the proposal would encourage street activity, this is considered positive, contributing to the vitality and life of

the adjacent civic space. Similarly, the location of the restaurant on the edge of the public space is considered appropriate and in accordance with the principles of good urban design, providing activity and overlooking to Tottenham Green.

- 6.21 Concerns have also been expressed regarding overlooking of neighbouring businesses. No additional windows are proposed as part of this application and thus the existing situation remains unchanged. In addition, a degree of overlooking within an urban context is considered normal and an acceptable scenario in this context.
- 6.22 Given the above, the proposed change of use does not have any significant impact on the amenity and operations of nearby homes and businesses and the application is in accordance with saved UDP policy UD3 General Principles.

Change of Use

- 6.23 Saved UDP policy TCR5: 'A3 restaurants and cafes, A4 drinking Establishments and A5 hot food takeaways states that 'The Council when assessing proposals for restaurants, cafes, drinking establishments and hot food takeaway uses that fall within A3, A4 and A5 use classes will take into account the following:
- a) the effectiveness of measures to mitigate litter, undue smell, odours and noise from the premises;
 - b) the hours of opening, operation and delivery; and
 - c) where appropriate the proportion of existing A3, A4 and A5 uses within the main town centres.

6.24 The Old Fire Station is currently occupied at ground floor level by a reception area to manage deliveries to the tenants and also as a front face for LYST (London Youth Support Trust) and the TGEC (Tottenham Green Enterprise Centre). The change of use to a restaurant at ground floor level is considered to positively contribute to the life and vitality of the Green, providing an active frontage, encouraging street life and activity within the public realm. Given that the proposal does not replace an A1 Shop use and is not located within a town centre, the proposal is considered to be in accordance with saved UDP policy TCR5. The proposed hours of operation are considered acceptable, (particularly given the distance from residential properties) and have been included as a condition of this planning permission. Permission is also subject to a condition relating to noise mitigation. Given the modest scale of the proposal no objection has been received from LBH Transportation regarding deliveries and parking.

Parking

- 6.25 The application site falls within an area that has a high public transport accessibility level of 6 and is served by a number of local bus services available on High Road Tottenham. The site is also within walking distance of Seven Sisters underground and rail station. The residential streets within the

immediate vicinity of the site are covered by the Seven Sisters controlled parking zone which operates Monday to Saturday 8:00am 6:30pm and offers a good level of on-street parking control.

- 6.26 It is considered that the proposal is likely to attract individuals from the local area or those using sustainable modes of transport to travel to the site. Members of staff are also likely to use public transport to travel to and from the site. It is unlikely that the above proposal will have any significant impact on the local transportation network. There are no objections from the LBH Transportation department.

Conclusion

- 6.27 This proposal to provide innovative healthy eating is welcomed and supported in principle.
- 6.28 The proposed scheme as assessed by the Council's Conservation Officer is acceptable, both preserving and enhancing the appearance of the Grade II listed building as well as other heritage assets within the immediate vicinity and the wider conservation area and not causing harm.
- 6.29 The proposed change of use is acceptable and does not have a significant, detrimental impact on the amenity of the neighbouring properties. Conditions have been applied regarding noise and opening hours. There are no concerns regarding the change of use or implications for parking and deliveries. The application is therefore considered in accordance with The National Planning Policy Framework (NPPF), Policy 7.8 'Heritage Assets and Archaeology' of the London Plan, Haringey Local Plan Policy SP12 and saved UDP policies CSV4 'Conservation', TCR5 'A3 restaurants and cafes, A4 drinking Establishments and A5 hot food takeaways' and UD3 'General Principles'
- 6.30 All other relevant policies and considerations, including equalities, have been taken into account. Planning permission should be granted for the reasons set out above. The details of the decision are set out in the RECOMMENDATION

7.0 RECOMMENDATIONS

GRANT PERMISSION subject to conditions

Applicant's drawing No.(s)

- 01-101 Site Plan As Existing 1:200 A1 * A
- 01-111 Ground Floor Plan As Existing 1:100 A1 * A
- 01-112 Public Realm Plan As Existing 1:100 A1 * A
- 01-121 Section AA As Existing 1:50 A1 * A
- 01-122 Section BB As Existing 1:50 A1 * A
- 01-131 Elevation AA As Existing 1:100 A1 * A
- 01-132 Elevation BB As Existing 1:100 A1 * A

01-133 Elevation CC As Existing 1:100 A1 * A
01-141 Site Elevation CC As Existing 1:200 A1 * A
01-201 Site Plan As Proposed 1:200 A1 * A
01-211 Ground Floor As Proposed 1:100 A1 * A
01-212 Public Realm As Proposed 1:100 A1 * A
01-301 Section AA As Proposed 1:50 A1 * A
01-302 Section BB As Proposed 1:50 A1 * A
01-401 Elevation AA As Proposed 1:100 A1 * A
01-402 Elevation BB As Proposed 1:100 A1 * A
01-403 Elevation CC As Proposed 1:100 A1 * A
01-404

Subject to the following condition(s)

1. The development hereby authorised must be begun not later than the expiration of 3 years from the date of this permission, failing which the permission shall be of no effect.

Reason: This condition is imposed by virtue of the provisions of the Planning & Compulsory Purchase Act 2004 and to prevent the accumulation of unimplemented planning permissions.

2. The development hereby authorised shall be carried out in accordance with the following approved plans and specifications: 01-201 Site Plan As Proposed 1:200 A1 * A, 01-211 Ground Floor As Proposed 1:100 A1 * A, 01-212 Public Realm As Proposed 1:100 A1 * A, 01-301 Section AA As Proposed 1:50 A1 * A, 01-302 Section BB As Proposed 1:50 A1 * A, 01-401 Elevation AA As Proposed 1:100 A1 * A, 01-402 Elevation BB As Proposed 1:100 A1 * A, 01-403 Elevation CC As Proposed 1:100 A1 * A 01-404

Reason: In order to avoid doubt and in the interests of good planning.

3. The use hereby permitted shall not be operated before 07:00 hours or after 23:30 hours Monday to Saturday and 10:00 and 19:00 on Sundays and Bank holidays.

Reason: This permission is given to facilitate the beneficial use of the premises whilst ensuring that the amenities of adjacent properties are not diminished consistent with Saved Policy UD3 of the Haringey Unitary Development Plan 2006.

4. Any noise generated by virtue of this development shall not cause an increase in the pre-existing background noise level or more than 5db (A). In this context, the background level is construed as measuring the level of noise which is exceeded for 90% of the time.

Reason: In order to protect the amenities of nearby residential occupiers consistent with Policy 7.15 of the London Plan 2011 and Saved Policy UD3 of the Haringey Unitary Development Plan 2006.

5. At no time shall any amplified speech or music generated from the site be audible within the nearby residential premises.

To prevent loss of amenity to neighbouring residential premises due to noise generated from the premises in accordance with Policy 7.15 of the London Plan 2011 and Saved Policy UD3 of the Haringey Unitary Development Plan 2006 and consistent with advice in the National Planning Policy Framework.

6. All works should be made good to match the existing fabric in colour, material and texture. If works cause any un-intentional harm to the existing fabric, this should be repaired or replicated to match existing.

Reason: To maintain the architectural and historical integrity of the listed building

7. Any hidden historic features (internal or external) which are revealed during the course of works shall be retained in situ, work suspended in the relevant area of the building and the Council as local planning authority notified immediately. Provision shall be made for the retention and/or proper recording, as required by the Local Planning Authority.

Reason: To ensure the correct preservation, enhancement and recording of historic features

8. Further details on all internal fixtures, external signage and fixtures, up lighters, steel pole and other public realm works should be submitted to the Council for approval.

Reason: In order to protect the character and setting of the listed building and the wider conservation area.

INFORMATIVES

Environmental Health

1. It is particularly important to install the correct type of filters for the type of cooking that you do. The Extractor Ventilation and Ducting System must be adequate to provide clean air and adequate ventilation for the cooking load, and safely remove any gases and heat without causing any hazards to the premises

and others. The Extractor and Ventilation Ducting must be manufactured in the materials recommended by the Building Control Officer and Building Regulations, and the London Fire and Emergency Planning Authority.

2. You must keep a written record of the Cleaning Schedule for the Extractor Ventilation and Ducting System that is available at all times for inspection by the Local Authority. It is further recommended that you include the Cleaning Schedule for the Extractor Ventilation and Ducting System within the daily records of the Cleaning Schedule for the Business.
3. Fumes resulting from cooking operations should be discharged via a mechanical extraction system so as not to create a hazard or a nuisance. The system should incorporate a stainless steel canopy connected to a flue and suitable filtration (i.e. grease and carbon etc.) in accordance with current regulations.
4. You must ensure the Engineer has installed anti vibration mountings/dampers to the fan or motor unit to prevent noise and vibration being transmitted into structure.
5. The Engineer must ensure flexible couplings are fitted between the fan unit and ducting to prevent vibration being transmitted into ducting
6. You must ensure the Flue Terminal at your business is positioned to prevent odour nuisances, and the risk of an outbreak of fire due to the lack of cleaning or due to other defects with the Extractor Ventilation and Ducting System as recommended by the Building Control Officer and Building Regulations, and the London Fire and Emergency Planning Authority.

Appendix 1: Consultation Responses from internal and external agencies

No.	Stakeholder	Question/Comment	Response
1.	INTERNAL	<p><u>Environmental Health</u> – It is particularly important to install the correct type of filters for the type of cooking that you do. The Extractor Ventilation and Ducting System must be adequate to provide clean air and adequate ventilation for the cooking load, and safely remove any gases and heat without causing any hazards to the premises and others. The Extractor and Ventilation Ducting must be manufactured in the materials recommended by the Buildin Control Officer and Building Regulations, and the London Fire and Emergency Planning Authority.</p> <p>2. You must keep a written record of the Cleaning Schedule for the Extractor Ventilation and Ducting System that is available at all times for inspection by the Local Authority. It is further recommended that you include the Cleaning Schedule for the Extractor Ventilation and Ducting System within the daily records of the Cleaning Schedule for the Business.</p> <p>3. Fumes resulting from cooking operations should be discharged via a mechanical extraction system so as not to create a hazard or a nuisance. The system should incorporate a stainless steel canopy connected to a flue and suitable filtration (i.e. grease and carbon etc.) in accordance with current regulations.</p> <p>4. You must ensure the Engineer has installed anti vibration mountings/dampers to the fan or motor unit to</p>	<p>The following condition has been added. The Environmental Health requirements opposite are included as an informative</p> <p>1. The use hereby permitted shall not be operated before 07:00 hours or after 23:30 hours Monday to Saturday and 10:00 and 19:00 on Sundays and Bank holidays.</p> <p>Reason: This permission is given to facilitate the beneficial use of the premises whilst ensuring that the amenities of adjacent properties are not diminished consistent with Saved Policy UD3 of the Haringey Unitary Development Plan 2006.</p> <p>2. Any noise generated by virtue of this development shall not cause an increase in the pre-existing background noise level or more than 5db (A). In this context, the background level is construed as measuring the level of noise which is exceeded for 90% of the time.</p> <p>Reason: In order to protect the amenities of nearby residential</p>

No.	Stakeholder	Question/Comment	Response
		<p>prevent noise and vibration being transmitted into structure.</p> <p>5. The Engineer must ensure flexible couplings are fitted between the fan unit and ducting to prevent vibration being transmitted into ducting</p> <p>6. You must ensure the Flue Terminal at your business is positioned to prevent odour nuisances, and the risk of an outbreak of fire due to the lack of cleaning or due to other defects with the Extractor Ventilation and Ducting System as recommended by the Building Control Officer and Building Regulations, and the London Fire and Emergency Planning Authority.</p>	<p>occupiers consistent with Policy 7.15 of the London Plan 2011 and Saved Policy UD3 of the Haringey Unitary Development Plan 2006.</p> <p>3. At no time shall any amplified speech or music generated from the site be audible within the nearby residential premises.</p> <p>To prevent loss of amenity to neighbouring residential premises due to noise generated from the premises in accordance with Policy 7.15 of the London Plan 2011 and Saved Policy UD3 of the Haringey Unitary Development Plan 2006 and consistent with advice in the National Planning Policy Framework.</p>
2.	LBH Transportation	Restaurant will increase parking pressure within the vicinity	The application site falls within an area that has a high public transport accessibility level of 6 and is served by a number of local bus services available on High Road Tottenham. The site is also within walking distance of Seven Sisters underground and rail station. The residential streets within the immediate vicinity of the site are covered by the Seven Sisters controlled parking zone

No.	Stakeholder	Question/Comment	Response
			<p>which operates Monday to Saturday 8:00am 6:30pm and offers a good level of on-street parking control.</p> <p>It is considered that the proposal is likely to attract individuals from the local area or those using sustainable modes of transport to travel to the site. Members of staff are also likely use public transport to travel to and from the site. It is unlikely that the above proposal will have any significant impact on the local transportation network. Therefore, there are no objections on highway and transportation grounds</p>
3.	Tottenham CAAC	Concerns regarding the impact that this might have on the listed buildings and conservation area.	<p>Conservation Officer Comment.</p> <p>In order to convert the building, the scheme proposes to remove the existing partitions, suspended ceilings. These currently detract from the building and their removal would allow the appreciation of the entire volume of the space, a significant feature pertaining to the original architecture and function of the building. As such the internal works would positively enhance the significance of the building and would be acceptable. Other works required for the operation of the restaurant would not have any impact on the fabric of the listed building and would be acceptable.</p>

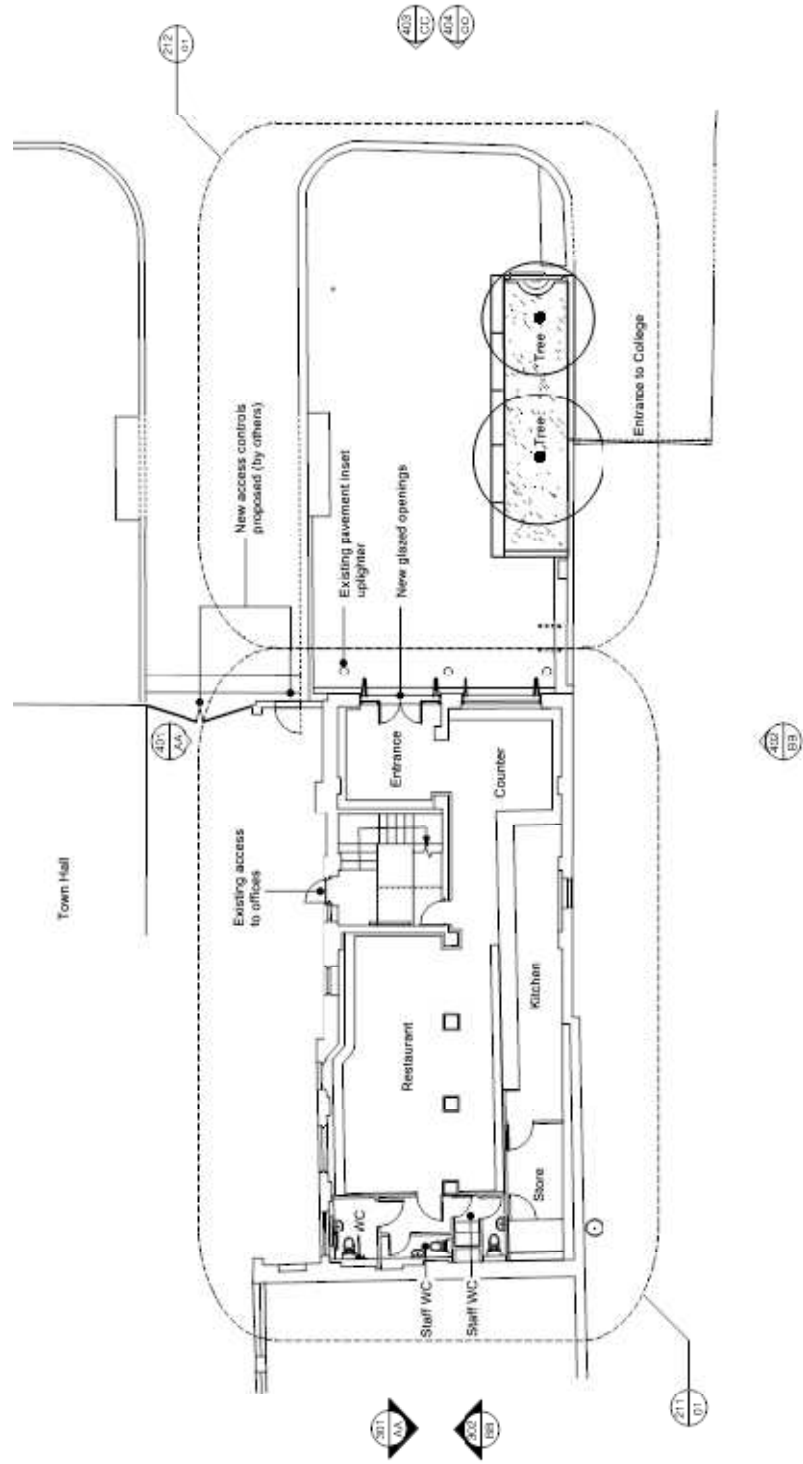
No.	Stakeholder	Question/Comment	Response
			<p>The new glazed entrance would replace a modern intervention and would have no impact on the fabric of the listed building. The extract required for the operations of the new use is proposed to be installed on the southern elevation. This elevation is of very little significance architecturally and already has a myriad of later fixtures including an existing vent pipe. As such the proposed duct would cause no additional impact to the fabric of the building and would be acceptable.</p> <p>The scheme also proposes works to the immediate paved area around it. Existing car parking is proposed to be removed. There would be tree planters, some up lighters and signage along with a gold anodised steel post with small sculpture. The works would have no impact on the fabric of listed building but would have some impact on its setting. Given the nature of the existing immediate public realm, the proposed works would be considered to enhance the building's setting as well as the Town Hall and the wider conservation area. Overall, the proposed scheme would be considered acceptable. In coming to this conclusion I have given great regard to the desirability of the preservation or enhancement of the heritage assets, as per the Council's statutory duty. The proposed</p>

No.	Stakeholder	Question/Comment	Response
			alterations to the building as well as to the setting rear would preserve and enhance the appearance of the Fire Station as well as other heritage assets within the immediate vicinity and would be acceptable.
4.	Local Residents	<p>For residents of Isobel Place;</p> <ul style="list-style-type: none"> • Objections from Isobel Place primarily relate to the impact of the restaurant on the immediate environment and anti-social behaviour issues that might arise. • Clarification required regarding the proposed use • Concern regarding the over spill of customers into Isobel Place 	<ul style="list-style-type: none"> • All applications for restaurant use are given conditions which the operators must adhere to, regarding ventilation, waste and fumes, to protect local resident and business amenity. The Chicken Town offer is designed to appeal to a mix of different groups of people and this diverse customer base will reduce the likelihood of anti-social behaviour being a problem. • The application is for a restaurant use (A3), not a hot food takeaway (A5), so the nature of its operation will be different to takeaways that are more typically associated with anti-social behaviour. • Newlon Housing which manages Isobel Place has been granted planning permission to install a

No.	Stakeholder	Question/Comment	Response
		<p>For business tenants within the Fire Station building;</p> <ul style="list-style-type: none"> • Tenants that have objected have voiced concerns about the operational matters, should the planning application be approved. The main function of the current reception area at the front of the Old Fire Station is to manage deliveries to the tenants and also as a front face for LYST and the TGEC. • A new restaurant use will increase parking pressure in the vicinity. 	<p>vehicular and pedestrian gate at the entrance to the housing, which would significantly reduce any potential impact for</p> <ul style="list-style-type: none"> • The continuity of these two key functions and alternative areas for managing deliveries and having a point of contact for tenants are being considered by all stakeholders involved (LYST, Chicken Town, CONEL and the Council). • Financial and technical support has been offered to LYST by Create¹ on these matters, for example through designing upgraded signage for the front of the building. • The plans for Chicken Town will improve the immediate area by prohibiting car parking in front of the Old Fire Station. This will be complemented by the Newlon

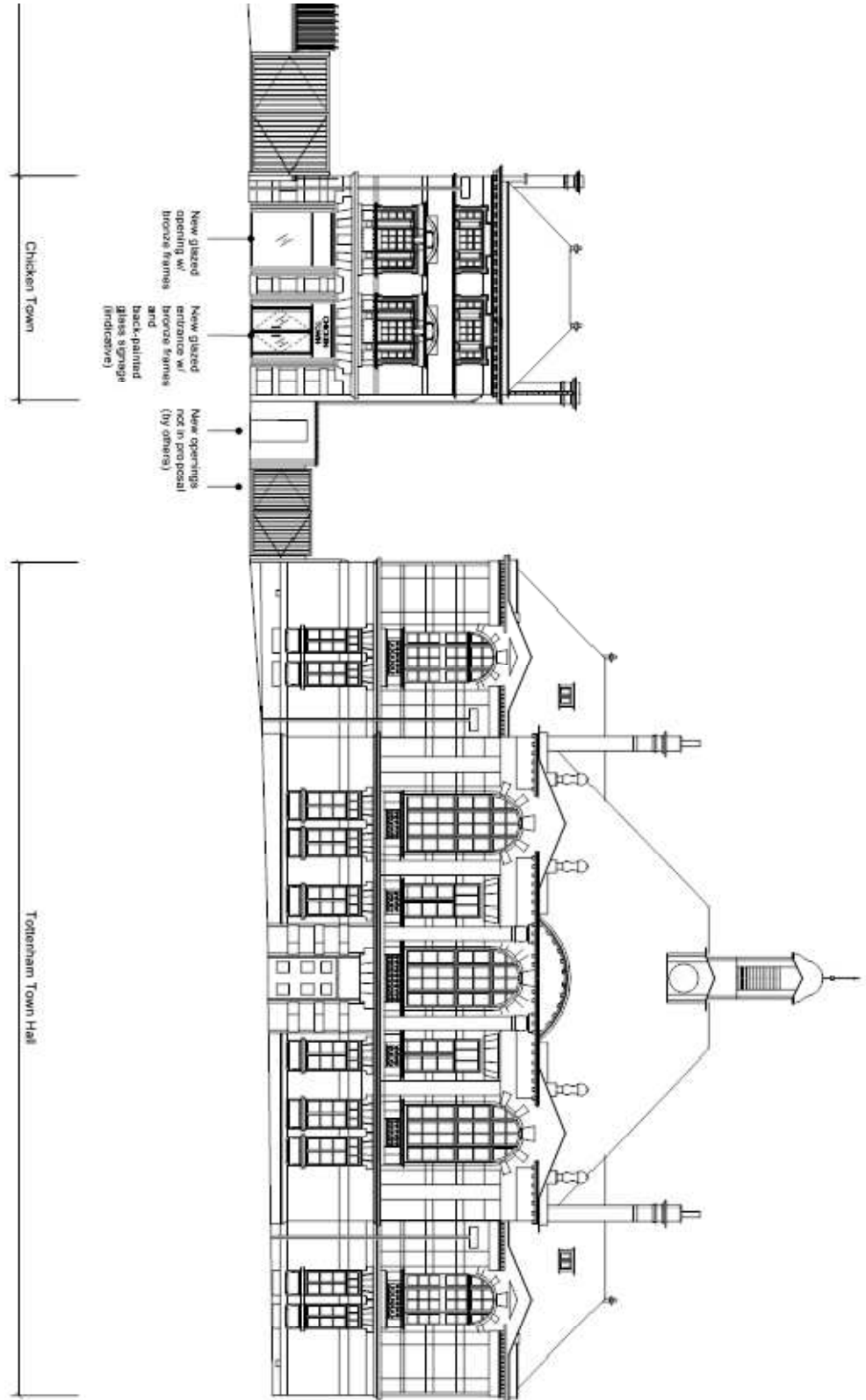
¹ Create is the arts organisation which is the commissioning body for Chicken Town.

No.	Stakeholder	Question/Comment	Response
			<p>Housing vehicular gates, which once installed will prevent cars parking to the side of the building as well.</p>



Drawing 1: Site Plan

Drawing 2: Proposed restaurant in relation to Tottenham Town Hall



Drawing 3: Front Elevation as Proposed

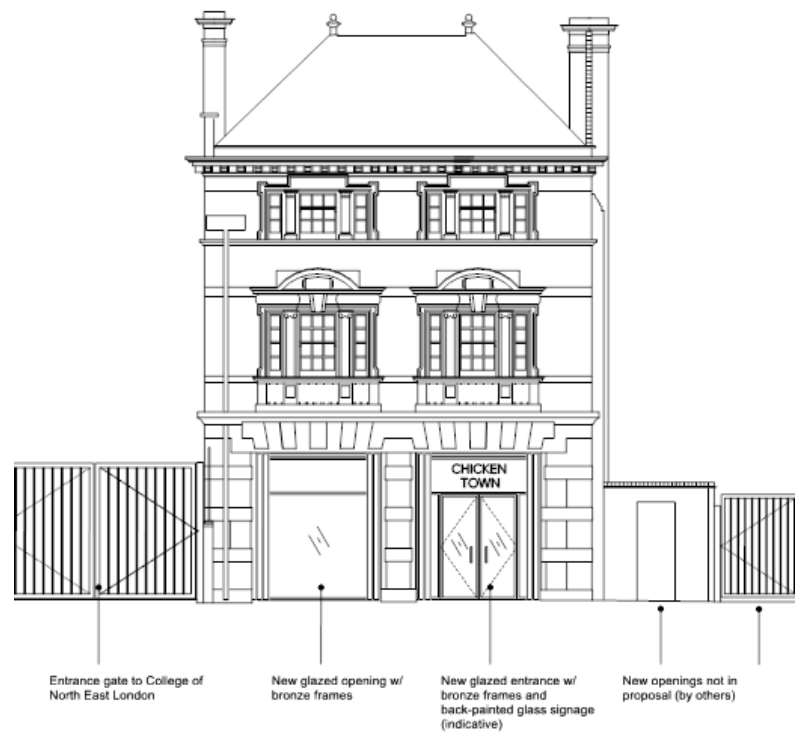


Photo 1: Fire Station operating within its intended use

Planning Sub Committee 23 July 2015

Item No.

REPORT FOR CONSIDERATION AT PLANNING SUB-COMMITTEE

1. APPLICATION DETAILS	
Reference No: HGY/2015/1512	Ward: Seven Sisters
Address: Stamford Hill Primary School Berkeley Road N15 6HD	
Proposal: Installation of a new working kitchen to cater for primary school children at Stamford Hill Primary School. Works involve the installation of new cooking appliances and an extract canopy with flue ducting to the outside. (retrospective application)	
Applicant: Ms Dianne Grant (Haringey Council)	
Ownership: Council	
Case Officer Contact: Samuel Uff	
Date received: 19/05/2015	
Drawing number of plans: E15352-CAN/A & E15352/F	
1.1 This application has been brought to committee because the Council is the applicant.	
1.2 SUMMARY OF KEY REASONS FOR RECOMMENDATION	
<ul style="list-style-type: none"> The school is required to make provision for hot food cooking facilities on-site and the associated ducting is considered acceptable in terms of its impact on the appearance of the building and surrounding area and on neighbouring amenity. 	
2. RECOMMENDATION	
That the Committee resolve to GRANT planning permission and that the Head of Development Management is delegated authority to issue the planning permission and impose conditions:	
Planning Application	
1) In accordance with plans, including colour of ducting	
2) Odour mitigation	
3) Noise mitigation	
In the event that member choose to make a decision contrary to officers' recommendation members will need to state their reasons.	

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3.0	PROPOSED DEVELOPMENT AND SITE LOCATION DETAILS
4.0	CONSULTATION RESPONSE
5.0	LOCAL REPRESENTATIONS
6.0	MATERIAL PLANNING CONSIDERATIONS <ul style="list-style-type: none"> • Impact on Residential Amenity
7.0	RECOMMENDATION
9.0	APPENDICES
	Appendix 1: Plans and images

3. PROPOSED DEVELOPMENT AND LOCATION DETAILS

Proposed development

- 3.1 The application seeks retrospective planning permission for the installation of an extract duct and stainless steel flue. The flue has a canopy system located within the building, which exits through the top of a large ground floor kitchen window in the northern elevation of the main school building. This fronts the junction of Seven Sisters Road and Berkeley Road.
- 3.2 The flue exits the window at a right angle for approximately 5m, at which point it angles straight up for an additional 6m, exiting approximately 1m above eaves height, via a jet cowl.
- 3.3 The installation of the kitchen use on site is in direct response to the Department for Education 'Universal infant free school meals' (Updated September 2014) advice, in response to the school's obligation to provide free school meals to all infants in reception, year 1 and year 2.

Site and Surroundings

- 3.4 The application site comprises a part single storey, part two storey school building with a variety of pitched roofs, prominent gable ends and modest dormers. The building occupies a large site between Seven Sisters Road, Berkely Road and Vartry Road, with associated play area and bounded by brick wall, palisade fencing or a combination of both. The site is not within a conservation area and the building is not locally or statutory listed.

Relevant Planning and Enforcement history

3.5 Planning History

HGY/2006/0135 GTD 14-03-06: Erection of 3.9m high jumbo timber gazebo in playground.

HGY/2008/0783 GTD 27-05-08: Removal of existing perimeter fencing to boundary wall, removal of existing brick piers and erection of new palisade

fence together with modification of existing pedestrian access to Berkeley Road and provision of new call entry panel.

4. CONSULTATION RESPONSE

4.1 The following were consulted regarding the application:

- LBH Building Control
- LBH Environmental Health

4.2 The following responses were received:

- LBH Building Control – no objection
- LBH Environmental Health – a series of requirements were requested, but it was later agreed that standard conditions, relating to noise and odour mitigation, would suffice.

5. LOCAL REPRESENTATIONS

5.1 The application was publicised by way of 13 letters to neighbouring properties and a site notice and no representations were received.

6. MATERIAL PLANNING CONSIDERATIONS

6.1 The main issues in respect of this application are considered to be:

- Principle of development
- Impact on the appearance of the building and on the surrounding area;
- Impact on amenity.

Principle of Development

6.2 Within the NPPF the Government attaches great importance to ensuring that a sufficient choice of school places is available to meet the needs of existing and new communities. This goes on to state that “Local planning authorities should take a proactive, positive and collaborative approach to meeting this requirement, and to development that will widen choice in education”. Furthermore this goes on to state “that local planning authorities work with schools promoters to identify and resolve key planning issues before applications are submitted.”

6.3 The applicant has emphasised the need for the improved kitchen facilities and the Government requirement for hot meals for primary school students. The school have been meeting this provision through outside catering deliveries.

Given the requirement for schools to provide this service on-site it is considered appropriate that associated ducting of some form and siting be installed, subject to design and amenity considerations.

Impact on the appearance of the building and surrounding area

- 6.4 Local Plan Policy SP11 states that all new development should enhance and enrich Haringey's built environment and create places and buildings that are high quality, attractive, sustainable, safe and easy to use. Development shall be of the highest standard of design that respects its local context and character and historic significance, to contribute to the creation and enhancement of Haringey's sense of place and identity which is supported by London Plan Policies 7.4 and 7.6.
- 6.5 The route of the ducting through the large central window in the northern elevation results in additional ducting along the external facade of the building. It was considered that routing this part internally would help minimise the visual impact, but this would have involved directing the ducting through classroom walls, ceilings and removing part of the load bearing wall. The route through the large window is preferable in that no additional openings or internal disruption is required. Furthermore, the removal of the window pane to allow this can be easily altered back to the original form, unlike the partial removal of this external wall. As such the impact on the host building is considered acceptable.
- 6.6 The extract ducting and flue are already in place on the northern elevation, facing Seven Sisters Road and Berkely Road. The siting of the host school building within the site means that this installation is significantly away from public highways; approximately 37m from the closest part of Seven Sisters Road and 47m from Berkely Road. Furthermore the palisade boundary fence, trees and vegetation further restrict views of this. The existing structure is a stainless steel material, which adds to its prominence. In this regard it is considered that a darker coloured finish, more in-keeping with the dark brick building, would help mitigate the visual impact of the flue, to the benefit of the appearance of the building and the surrounding area. The applicant has agreed to alter the colour of the flue. Submission of details of the revised colour is conditioned.
- 6.7 In the context of the policy and impact on this non-listed building, outside of a conservation area, the development is considered necessary for the day to day functioning of this school and this is considered sufficient justification for the development.
- 6.8 The route of the ducting through the large window means that the integrity of the brick wall of the building is not compromised and that this can be removed without damaging the long term appearance of the building. Given the significant distance from public highway, combined with the presence of the palisade fencing and vegetation and the amended darker, less visually intrusive finish, it is considered that the visual impact would not be significant. As such the proposal accords with London Plan polices 7.4 and 7.6 and the NPPF.

Impact on residential amenity

- 6.11 The London Plan 2011 policy 7.6 'Architecture' states that development must not cause unacceptable harm to the amenity of surrounding land and buildings. Saved Policy UD3 also requires development not to have a significant adverse impact on residential amenity.
- 6.12 There is at least 55m between the flue and the nearest properties and as such there is not considered to be any impact on the amenity of neighbouring properties. In any case a conditions restricting noise has been included on the decision notice.

Conclusion

- 6.13 The school is required to make provision for hot food cooking facilities on-site and the associated ducting is considered acceptable in terms of its impact on the appearance of the building and surrounding area and on neighbouring amenity.
- 6.14 Planning permission should be granted for the reasons set out above. The details of the decisions are set out in the RECOMMENDATION below.

7.0 RECOMMENDATIONS

- 7.1 GRANT PLANNING PERMISSION (HGY/2015/0734) subject to conditions

Applicant's drawing No.(s) E15352-CAN/A & E15352/F

Subject to the following condition(s)

1. Notwithstanding the information submitted with this application, details of the amended flue colouring and how the works will be undertaken and maintained, shall be submitted to and approved in writing within 3 months of this decision date and implemented in accordance with the requirements of the Local Planning Authority and retained as such in perpetuity.

Reason: In order to ensure the development is carried out in accordance with the approved details and to safeguard the character and appearance of the building and surrounding area, consistent with Policy SP11 of the Haringey Local Plan 2013 and Saved Policy UD3 of the Haringey Unitary Development Plan 2006.

2. Notwithstanding the approved plans, equipment to control the emission of fumes and smell from the premises shall be installed in accordance with a scheme to be first submitted to and approved in writing by the Local Planning Authority. All equipment installed as part of the approved scheme shall

thereafter be operated and maintained in accordance with that approval and retained for so long as the ancillary kitchen use continues.

Reason: In order to protect the amenities of nearby residential occupiers consistent with Policy 7.15 of the London Plan 2011 and Saved Policy UD3 of the Haringey Unitary Development Plan 2006.

3. The design and installation of new items of fixed plant hereby approved by this permission shall be such that, when in operation, the cumulative noise level LAeq 15 min arising from the proposed plant, measured or predicted at 1m from the facade of nearest residential premises shall be a rating level of at least 5dB(A) below the background noise level LAF90. The measurement and/or prediction of the noise should be carried out in accordance with the methodology contained within BS 4142: 1997. Upon request by the local planning authority a noise report shall be produced by a competent person and shall be submitted to and approved by the local planning authority to demonstrate compliance with the above criteria.

Reason: In order to protect the amenities of nearby residential occupiers consistent with Policy 7.15 of the London Plan 2011 and Saved Policy UD3 of the Haringey Unitary Development Plan 2006

APPENDIX 1

Site Location Plan



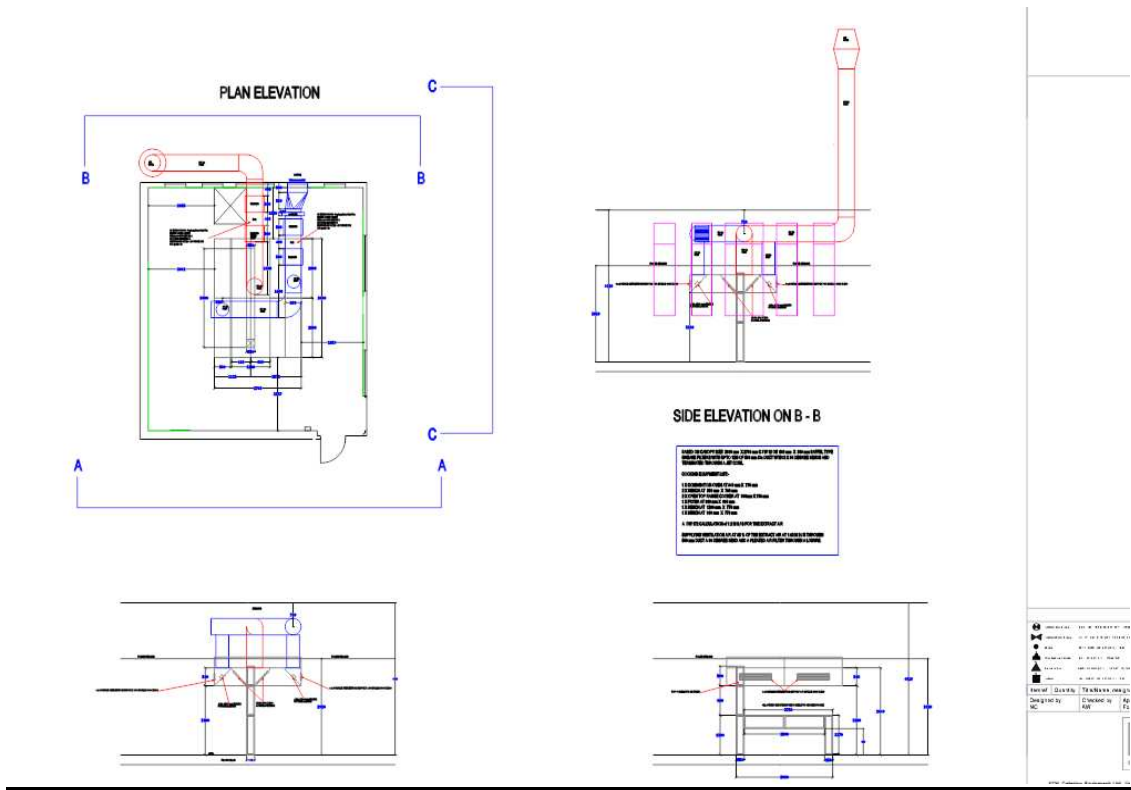
Photo 1 – View from the playground



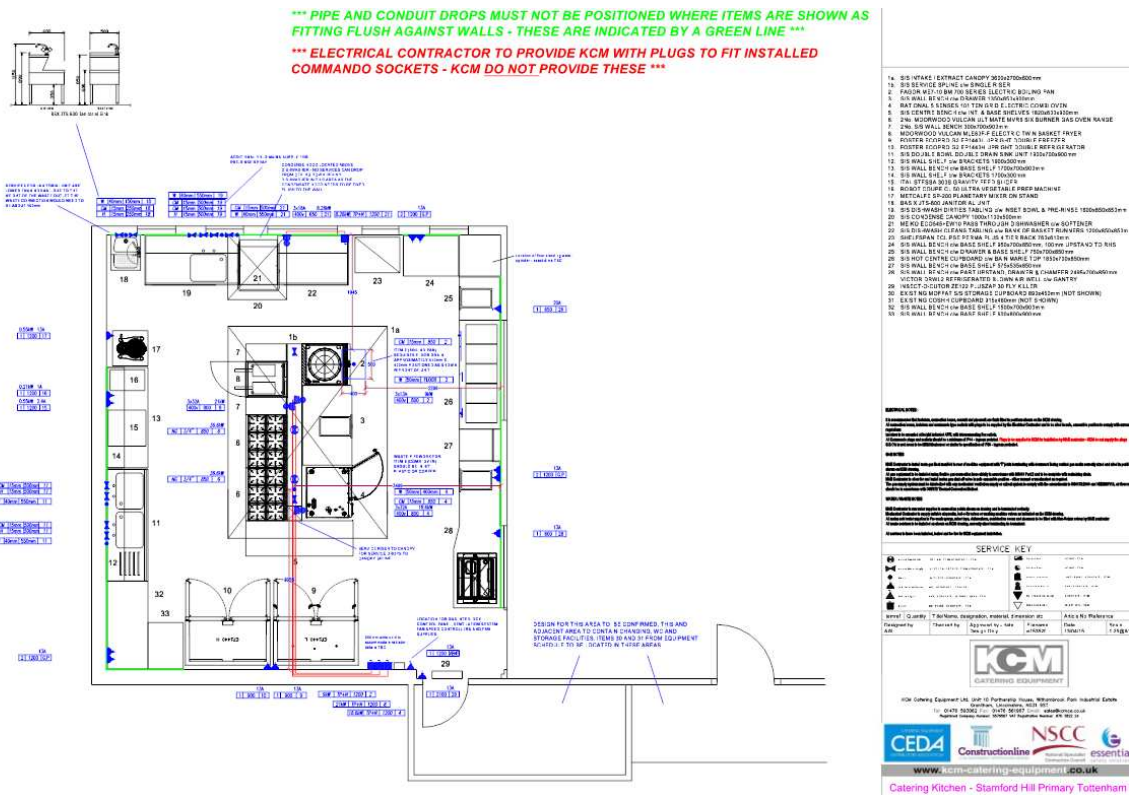
Photo 2 – View from Seven Sisters Road



Drawing 1:



Drawing 2:



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Planning Sub Committee 23/07/2015

Item No.

REPORT FOR CONSIDERATION AT PLANNING SUB-COMMITTEE

1. APPLICATION DETAILS	
Reference No: HGY/2015/1279	Ward: Bounds Green
Address: Bounds Green Junior and Infant School Bounds Green Road N11 2QG	
Proposal: Demolition of the existing wing adjacent to Block 4 Key Stage 2 building facing Bounds Green Road and replacement with a two storey extension building including new classroom and teaching rooms. Works also including a new single storey small extension to Block 1 kitchen block to the rear side (northwest) of the site and resurfacing/alterations to external playground	
Applicant: Bounds Green School	
Ownership: Council	
Case Officer Contact: Robbie McNaugher	
Site Visit Date: 21/05/2015	
Date received: 01/05/2015	
Drawing number of plans: 122.RLA.BG.101A, 600, 601, 602, 041, 042, 043, 044, 045, 046, 047, 048, 049, 050, 051, 052.	
1.1	This application has been brought to committee because the Council is the applicant.
1.2	SUMMARY OF KEY REASONS FOR RECOMMENDATION
	<ul style="list-style-type: none"> • The principle of increased educational facilities is welcomed • The design and appearance of the proposal is acceptable • The impact of the development on neighbouring residential amenity is acceptable • There would be no significant impact on traffic movements or parking locally

2. RECOMMENDATION

That the Committee resolve to GRANT planning permission and that the Head of Development Management is delegated authority to issue the planning permission and impose conditions and informatives

Conditions

- 1) Development begun no later than three years from date of decision
- 2) In accordance with approved plans
- 3) Materials submitted for approval
- 4) Travel Plan
- 5) Pedestrian crossing
- 6) Drainage
- 7) Construction dust
- 8) Boilers

Informatives

- 1) Positive statement
- 2) Hours of construction
- 3) Asbestos

In the event that member choose to make a decision contrary to officers' recommendation members will need to state their reasons.

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3.0 PROPOSED DEVELOPMENT AND LOCATION DETAILS

3.1 Proposed development

This is an application for the demolition of the existing wing adjacent to Block 4 Key Stage 2 building facing bounds Green Road and replacement with a two storey extension building including new classroom and teaching rooms to allow an increase from two to three form entry. Works also including a new single storey small extension to Block 1 kitchen block to the rear side (northwest) of the site and resurfacing/alterations to the external playground. The works will result in an increase from 509 students to 656 students over seven years including 26 full-time nursery places. The total increase will result in 147 additional pupils and 20 full time staff.

The proposed works include two extensions: one classroom block will occupy a prominent position on the southern corner of the site, fronting Bounds Green Road and Park Road; a second extension to the kitchen will occupy the northwest corner of the site, facing the school's parking area on one side and the back gardens of terraced housing on Durnsford Road on the other.

3.2 Site and Surroundings

The site is located on the eastern side of Bounds Green Road close to Bounds Green Underground Station. The school is situated in a largely residential area with two storey terraced housing surrounding it on three sides. Bounds Green Road is a busy A-road and a pedestrian walkway has been constructed overhead to provide access to the school's main entrance.

The existing school building was first built in the 1900s and has been altered and extended over time, arranged within the square site in an L-shape around the playground in the southwest corner.

3.4 Relevant Planning and Enforcement history

HGY/2000/0372 GTD 20-06-00 Bounds Green Junior & Infant School Bounds Green Road London Erection of part single/two story extension accommodating eight classrooms and dining hall to existing school and the erection of a temporary kitchen and washroom.

HGY/2003/1511 GTD 07-10-03 Bounds Green Junior & Infant School Bounds Green Road London Erection of 3m high palisade fencing and gate on Park Road frontage.

HGY/2003/1597 GTD 14-10-03 Bounds Green Junior & Infant School Bounds Green Road London Erection of new boundary wall and gates and associated improvements to playground.

HGY/2004/0691 GTD 27-04-04 Bounds Green Junior & Infant School Bounds Green Road London Provision of access ramp and alterations to elevations including replacement doors and windows and new openings.

HGY/2007/2450 GTD 15-01-08 Bounds Green Junior and Infant School, Bounds Green Road London Alterations and extension of existing school building to incorporate a new SureStart Children's Centre, new school entrance, new covered walkways, and new ramped entrance to the main school gates.

HGY/2007/2637 GTD 12-02-08 Planters adjacent to Bounds Green Junior and Infant School, Bounds Green Road London Display of 4 name plates advertising planter sponsorship and Haringey Council.

4. CONSULTATION RESPONSE

4.1 The following were consulted regarding the application:

LBH Transportation
LBH Waste
LBH Education
LBH Drainage
LBH Environmental Health

The following responses were received :

Internal:

1) Transportation

No objections subject to a revised travel plan, pedestrian crossing on Park Road and travel plan monitoring.

2) Drainage

No objections subject to details of the works proposed to unblock the drain marked: 'Manhole cover 22'

3) Education

The proposed works support the agreed delivery of a reinstatement of Bounds Green Infant and Junior School from two to three form entry and supports Education Service's plan to ensure continued school place sufficiency. In this respect we support the work proposed to achieve this.

4) Environmental Health Pollution

No objections subject to conditions for construction management and boiler emissions.

5. LOCAL REPRESENTATIONS

5.1 The application has been publicised by way of 135 letters

5.2 No responses were received

6 MATERIAL PLANNING CONSIDERATIONS

6.1 The main planning issues raised by the proposed development are:

1. Principle of the development
2. Design and appearance
3. The impact on the amenity of adjoining occupiers
4. Parking and highway safety
5. Trees

6.2 Principle of the development

Local Plan Policy SP0 supports the broad vision of the NPPF, and states that the Council will take a positive approach to reflect the presumption in favour of sustainable development. Permission will be granted by the Council unless any benefits are significantly outweighed by demonstrable harm caused by the proposal.

The NPPF recognises that the planning system can play an important role in facilitating social interaction and creating healthy, inclusive communities. London Plan policy 3.18 lends support to proposals that enhance education provision and serve to meet the demands of a growing population. Local Plan policy SP16 seeks to ensure the appropriate improvement and enhancements of community facilities.

This proposal serves to enhance the facilities on offer at the site, providing 3 new classrooms and new teaching rooms to allow an increase from two to three form entry and an extended kitchen facility and improved playground.

The scheme provides improved facilities at the school to the benefit of the pupils, staff and parents. The scheme accords with the Council's aims to support the provision of a high standard of education in the borough and is in accordance with policy SP16 of the Local Plan. As such, the proposal is acceptable in planning terms and is supported subject to the considerations set out below.

6.3 Design and appearance

Local Plan Policy SP11 states that all new development should enhance and enrich Haringey's built environment and create places and buildings that are high quality, attractive, sustainable, safe and easy to use. Development shall be of the highest standard of design that respects its local context and character and historic significance, to contribute to the creation and enhancement of Haringey's sense of place and identity, which is supported by London Plan Policies 7.4 and 7.6.

The proposal involves the demolition of the existing former Premises Manager's house, which is unsuitable for re-use and is of little architectural merit. The extension would be a contemporary 2 storey addition to the existing building at the southern corner fronting onto both Bounds Green Road and Park Road. Internally the building would provide good quality accessible teaching spaces with good natural light. It would be a contrasting design to the existing school building with a flat roof and modern fenestration. It would however reflect some of the architectural language and rhythm of the existing building with a strong vertical emphasis provided by tall windows and the use of 2 tones of brick and traditional downpipes and hoppers. The building

has been designed to remain subordinate to the existing building with a lower roof height and is joined to it by a small aluminium panel. The extension would be a noticeable addition to the streetscene at a visible location fronting onto 2 streets and visible at the rear. It is considered that the contemporary design and details would address these frontages to provide a high quality addition to the streetscene. A condition is recommended requiring the submission of material samples should the application be approved.

The proposed kitchen extension is small scale finished in brick with a flat roof and is also a high quality modern addition to this building.

The proposal is acceptable and in general accordance with London Plan 2011 Policies 3.5 and 7.6, and Local Plan 2013 Policy SP11.

6.4 Impact on the amenity of adjoining occupiers

The London Plan 2011 Policy 7.6 *Architecture* states that development must not cause unacceptable harm to the amenity of surrounding land and buildings. Saved Policy UD3 also requires development not to have a significant adverse impact on residential amenity in terms of loss of daylight, or sunlight, privacy overlooking, aspect noise, pollution and of fume and smell nuisance.

The extension would replace an existing building of a similar height and is set well in from the site boundaries so would not adversely impact on neighbouring properties. The proposed kitchen extension is single storey and adjacent to an existing car park so would not adversely impact on neighbouring properties.

The proposal therefore does not harm the amenities of neighbours and is in general accordance with saved UDP 2006 Policy UD3 and concurrent London Plan 2011 Policy 7.6.

6.5 Parking and highway safety

Local Plan (2013) Policy SP7 Transport states that the Council aims to tackle climate change, improve local place shaping and public realm, and environmental and transport quality and safety by promoting public transport, walking and cycling and seeking to locate major trip generating developments in locations with good access to public transport

The Council's Transportation Team has been consulted and advises that the school is located on the west side of Bounds Green Road with the junction of Park Road N11, the site falls within an area with a high public transport accessibility level (PTAL 5) and is some 150 metres from Bounds Green underground station. Bounds Green Road, the A109 is part of the Transport for London Strategic Road Network (SRN) and acts as an arterial link to the A406, as such Bounds Green Road has heavy traffic flows to and from the A406; at the time of the site visit traffic was observed travelling in excess of 30 mph.

The school has its primary pedestrian access from Bounds Green Road; the access is segregated from the carriageway by guard railings to prevent pedestrians crossing the

A109. There are signalised pedestrian crossing points at the junction of Bounds Green Road with Durnsford Road and Brownlow Road and the junction of Park Road with Bounds Green Road. The vehicular access to the school is via Park Road from Bounds Green and provides access to the car park and servicing for the school, the vehicular access will remain unchanged.

The applicant's consultant "Project Centre" has reviewed the last 3 years accident data for the roads surrounding the site. The results of the analysis concluded that there were some 35 accidents within the last 36 months, with a concentration of accidents at the junction of Bounds Green Road with the junction of Brownlow Road, which is to be expected. Of the 35 accidents, two of the accidents were serious and involved: 1) an adult cyclist on the pavement colliding with a vehicle exiting the petrol station and 2) a 15 year old male running between two stationary buses and colliding with a car that was overtaking. The remaining 33 accidents were recorded as slight injuries; two of the slight injuries involved children one at the junction of Bounds Green Road with Brownlow Road at lunch time and the other, an 11 year old boy who was crossing between two stationary cars on Durnsford Road on his way to school. No accidents were recorded in the last 36 months on Park Road or at the junction of Park Road with Bounds Green Road, which is the closest crossing point to the school.

The Council is proposing to reinstate Bounds Green Primary to a 3 form entry. The school currently has 2 form entry; the reinstatement will result in an increase from 509 students to 656 students over seven years including 26 full-time nursery places. The total increase will result in 147 additional pupils and 20 full time staff. The majority of pupils (86%) live in the surrounding N11 and N22 postcode areas and within approximately 2km of the school. As school place allocation is based on local catchment areas, the school's 2013 School Travel Plan suggests that some 92% of all pupils travel to school by sustainable modes of transport, with some 66% of pupils walking to school. The applicant's transport consultant (Project Centre) has forecasted that the proposed increase in pupils will result in approximately 628 additional journeys (person trips) made per day. Of these, 314 of these trips will occur in the morning peak as a result of the reinstatement of the school to a 3 form entry, 21 of these trips will be by car and 200 trips on foot when the school reaches full capacity.

The Transportation Team do not consider that the proposed increase of some 147 pupils will impact on the operation of the transportation and highways network; as the forecasted increase in car trips would be a maximum of some 21 additional car trips during the network's critical AM peak period hour.

They consider that 66% of pupils will walk to and from the site, and have reviewed the existing walking facilities in the area surrounding the site. There are two crossing points within 80 metres to the south and north of the site, and a pedestrian bridge over Bounds Green Road directly fronting the Bounds Green Road school entrance. The school Travel Plan has highlighted the crossing of Bounds Green Road at the junction with Durnsford Road and Brownlow Road as one of the main issues when walking to and from the school, as cars frequently go through on the green pedestrian phase, the accident data suggests that there is a high concentration of accidents at this junction. However, this junction has recently been upgraded and has pedestrian phases to facilitate pedestrian crossing at this junction. We have therefore considered that, this crossing combined with the two other crossings some 180 metres immediately south

of the junction of Bounds Green Road with Brownlow Road provide sufficient connectivity to link the east and west of Bounds Green Road to provide pedestrian access to the school.

The Transport Assessment suggests improvements to the Park Road access. We have no objection to improving this access point. Park Road has a width of 11 meters and is relatively heavily parked; there are school keep clear markings along the entire length of the school site. However, there are no formal crossing points onto Park Road, and the school travel plan has highlighted this as an issue. The applicant has proposed providing a formal crossing point with a raised zebra crossing with build outs to reduce the crossing distance and improve forward visibility as per Drawing 1000002233-2-110, the crossing point is to be secured as part of the planning permission.

The school currently has 28 car parking spaces, which are dedicated for staff use. The site is in an area with a high public transport accessibility level and given the proximity of the station to the school, some 150 metres, no additional car parking spaces will be required as part of this development proposal. It has been noted that the on-site cycle storage provision will be increased in order to meet London Plan standards.

Consequently, the transportation and highways authority would not object to this application subject to a revised travel plan, pedestrian crossing on Park Road and travel plan monitoring.

6.6 Trees

With regard to trees, UDP (2006) Policy OS17 states that the Council will seek to protect and improve the contribution of trees, tree masses and spines to local landscape character by ensuring that, when unprotected trees are affected by development, a programme of tree replanting and replacement of at least equal amenity and ecological value and extent is approved by the Council.

The proposal would not adversely impact on the root protection area of the mature trees on the site and is therefore considered acceptable in this respect.

6.7 Conclusion

The scheme accords with the Council's aims to support the provision of a high standard of education in the borough, the design and appearance of the proposal is considered to be high quality, and would not adversely impact on neighbouring amenity. There would be no significant impact on traffic movements or parking locally or trees.

All other relevant policies and considerations, including equalities, have been taken into account. Planning permission should be granted for the reasons set out above. The details of the decision are set out in the RECOMMENDATION

6.6 CIL

There is a NIL rate charge for schools under the Mayor's and Haringey's CIL charging schedule and therefore the proposal is not liable for the Mayor's or Haringey's CIL charge.

8.0 RECOMMENDATIONS

GRANT PERMISSION subject to conditions

Applicant's drawing No.(s) 122.RLA.BG.101A, 600, 601, 602, 041, 042, 043, 044, 045, 046, 047, 048, 049, 050, 051, 052.

Subject to the following condition(s)

1. The development hereby authorised must be begun not later than the expiration of 3 years from the date of this permission, failing which the permission shall be of no effect.

Reason: This condition is imposed by virtue of the provisions of the Planning & Compulsory Purchase Act 2004 and to prevent the accumulation of unimplemented planning permissions.

2. The development hereby authorised shall be carried out in accordance with the following approved plans and specifications:

122.RLA.BG.101A, 600, 601, 602, 041, 042, 043, 044, 045, 046, 047, 048, 049, 050, 051, 052.

Reason: In order to avoid doubt and in the interests of good planning.

3. Notwithstanding the information submitted with this application, no development shall take place until precise details of the external materials to be used in connection with the development hereby permitted be submitted to, approved in writing by and implemented in accordance with the requirements of the Local Planning Authority and retained as such in perpetuity.

Reason: In order to retain control over the external appearance of the development in the interest of the visual amenity of the area and consistent with Policy SP11 of the Haringey Local Plan 2013 and Saved Policy UD3 of the Haringey Unitary Development Plan 2006.

4. The-development hereby permitted shall not be occupied until such time as a revised travel plan has been provided to the Council's travel plan co-ordinator and an agreement has been reached to monitor the travel plan initiatives annually (at a cost of £3,000). The approved travel plan shall be implemented prior to first occupation of the development hereby permitted. The developer must submit a travel plan, annually for a period of no less than 3 years.

Reason: To promote sustainable transport and to reduce the potential for additional on street parking stress as a result of the development, consistent with Policies SP0, SP4 and SP7 of the Haringey Local Plan.

5. The development hereby permitted shall not be occupied until such time as the pedestrian crossing facilities on Park Road as per drawing 1000002233-2-110 have been implemented (at a cost of £51,235).

Reason: In the interests of the general safety of the highway and consistent with Policy 6.13 of the London Plan 2011 and Saved Policies UD3 and M10 of the Haringey Unitary Development Plan 2006.

6. Prior to the commencement of the development hereby permitted a drain blockage investigation shall be submitted to and approved in writing by the Local Planning Authority in respect of the blocked drain indicated as 'Manhole cover 22' on drawing number 1192 602 P2. The development shall be completed in accordance with the approved details.

Reason: To promote a sustainable development consistent with Policies SP0, SP4 and SP6 of the Haringey Local Plan 2013.

7. No works shall be carried out on the site until a detailed report, including risk assessment, detailing management of demolition and construction dust has been submitted and approved by the Local Planning Authority (reference to the London Code of Construction Practice) and that the site of contractor company be registered with the considerate constructors scheme. Proof of registration must be sent to the Local Planning Authority prior to any works being carried out on site.

Reasons: To safeguard the amenities of the area consistent with Policies 6.3, 6.11 and 7.15 of the London Plan 2011, Policies SP0 of the Haringey Local Plan 2013 and Saved Policy UD3 of the Haringey Unitary Development Plan 2006.

8. Prior to the first occupation of the development hereby approved installation details of the boiler to be provided for space heating and hot water are to be submitted to and approved in writing by the Local Planning Authority. The boilers to be provided for space heating and hot water shall have dry NOx emissions not exceeding 40mg/kWh (0%). The boilers are to be installed and permanently retained thereafter, or until such time as more efficient technology can replace those previously approved.

Reason: To reduce pollution, as required by the London Plan 2011 Policy 7.14.

INFORMATIVE 1: In dealing with this application, Haringey Council has implemented the requirements of the National Planning Policy Framework and of the Town and Country Planning (Development Management Procedure) (England) (Amendment No.2) Order 2012 to foster the delivery of sustainable development in a positive and proactive manner.

INFORMATIVE 2: Hours of Construction Work: The applicant is advised that under the Control of Pollution Act 1974, construction work which will be audible at the site boundary will be restricted to the following hours:-

- 8.00am - 6.00pm Monday to Friday
- 8.00am - 1.00pm Saturday
- and not at all on Sundays and Bank Holidays.

INFORMATIVE 3: Asbestos: Prior to demolition of existing buildings, an asbestos survey should be carried out to identify the location and type of asbestos containing materials. Any asbestos containing materials must be removed and disposed of in accordance with the correct procedure prior to any demolition or construction works carried out.

Appendix 1 Consultation Responses from internal and external agencies

No.	Stakeholder	Question/Comment	Response
	INTERNAL		
	Transportation	<p>The school is located on the west side of Bounds Green Road with the junction of Park Road N11, the site falls within an area with a high public transport accessibility level (PTAL 5) and is some 150 metres from Bounds Green underground station. Bounds Green Road, the A109 is part of the Transport for London Strategic Road Network (SRN) and acts as an arterial link to the A406, as such Bounds Green Road has heavy traffic flows to and from the A406; at the time of the site visit traffic was observed travelling in excess of 30 MPH.</p> <p>The school has its primary pedestrian access from Bounds Green Road, the access is segregated from the carriageway by guard railings to prevent pedestrians crossing of the A109. There are signalised pedestrian crossing points as the junction of Bounds Green Road with Durnsford Road and Brownlow Road and the junction of Park Road with Bounds Green Road. The vehicular access to the school is via Park Road from Bounds Green and provides access to the car park and servicing for the school, the vehicular access will remain unchanged.</p> <p>The applicant’s consultant “Project Centre” has reviewed the last 3 years accident data for the roads surrounding the site. The results of the analysis concluded that there were some 35 accidents within the last 36 months, with a concentration of accidents at the junction of Bounds</p>	Comments noted and conditions attached

No.	Stakeholder	Question/Comment	Response
		<p>Green Road with the junction of Brownlow Road which is to be expected. Of the 35 accidents two of the accidents were serious and involved: 1) an adult cyclist on the pavement colliding with a vehicle exiting the petrol station and 2) a 15 year old male running between two stationary buses and colliding with a car that was overtaking. The remaining 33 accidents were recorded as slight injuries; two of the slight injuries involved children one at the junction on of Bounds Green Road with Brownlow Road at lunch time and the other, a 11 year old boy who was crossing between two stationary cars on Durnsford Road on his way to school. No accidents were recorded in the last 36 months on Park Road or at the junction of Park Road with Bounds Green Road which is the closest crossing point to the school.</p> <p>The Council is proposing to reinstate Bounds Green Primary to a 3 form entry. The school currently has 2 form entry; the reinstatement will result in an increase from 509 students to 656 students after seven years including 26 full-time nursery places. The total increase will result in some 147 additional pupils and 20 full time staff. The majority of pupils (86%) live in the surrounding N11 and N22 post code areas and within approximately 2km of the school. As school place allocation is based on local catchment areas; the school's 2013 School Travel Plan suggests that some 92% of all pupils travel to school by sustainable modes of transport, with some 66% of pupils walking to school. The applicants transport consultant (Project Centre) has forecasted that the proposed increase in pupils will result in</p>	

No.	Stakeholder	Question/Comment	Response
		<p>approximately 628 additional journeys (person tips) made per day. Of these, 314 of these trips will occur in the morning peak as a result of the reinstatement of the school to a 3 form entry, 21 of these trips will be by car and 200 tips on foot when the school reaches full capacity.</p> <p>We do not consider that the proposed increase of some 147 pupils will impact on the operation of the transportation and highways network; as the forecasted increase in car trips would be a maximum of some 21 additional car tips during the networks critical AM peak period hour.</p> <p>Considering that 66% of pupils walking to and from the site, we have reviewed the existing walking facilities in the area surrounding the site. There are two crossing points within 80 metres to the south and north of the site, and a pedestrian bridge over Bounds Green Road directly fronting the Bounds Green Road school entrance. The school Travel Plan has highlighted the crossing of Bounds Green Road at the junction with Dunsford Road and Brownlow Road as one of the main issues when walking to and from the school, as cars frequently go through on the green pedestrian phase, the accident data suggest that there is a high concentration of accidents at this junction. However this junction has recently been upgraded and has pedestrian phases to facilitate pedestrian crossing at this junction. We have therefore considered that, this crossing combined with the two other crossings some 180 metres immediately</p>	

No.	Stakeholder	Question/Comment	Response
		<p>south of the junction of Bounds Green Road with Brownlow Road provide sufficient connectivity to link the east and west of Bounds Green Road to provide pedestrian access to the school.</p> <p>The Transport Assessment suggests improvements to the Park Road access. We have no objection to improving this access point. Park Road has a width of 11 meters and is relatively heavily parked, there are school keep clear marking site side long the entire length of the school site. However, there are no formal crossing points onto Park Road, and the school travel plan has highlighted this as an issue. The applicant has proposed providing a formal crossing point with a raised zebra crossing with build outs to reduce the crossing distance and improve forward visibility as per Drawing 1000002233-2-110, the crossing point is to be secured as part of the planning permission.</p> <p>The school currently has 28 car parking spaces, which are dedicated for staff use. The site is in an area with a high public transport accessibility level and given the proximity of the station to the school, some 150 metres, no additional car parking spaces will be required as part of this development proposal. It has been noted that the on-site cycle storage provision will be increased in order to meet London Plan standards.</p> <p>Consequently the transportation and highways authority would not object to this application subject to the imposition of the following conditions:</p>	

No.	Stakeholder	Question/Comment	Response
		<p>1. The developer must submit a revised school travel plan, annually for a period of no less than 3 years.</p> <p>Reason: To minimise the traffic impact of this development on the adjoining roads, and to promote travel by sustainable modes of transport.</p> <p>2. The applicant/developer will be required to contribute a sum of £51,235 (fifty one thousand two hundred and thirty five pounds) for the implementation of pedestrian crossing facilities on Park Road. The details of and funding for the proposed scheme must be secured and agreed in writing with the Highways authority prior to the first use of development hereby permitted.</p> <p>Reason: To provide enhance walking and in order to promote travel by sustainable modes of transport to and from the site.</p> <p>4. The applicant/ Developer will be required to contribute £3,000 (Three thousand pounds) for travel plan monitoring.</p> <p>Reason: To promote travel by sustainable modes of transport to and from the site, reduce traffic and congestion in the area surrounding the site.</p>	
	Drainage	No objections subject to details of the works proposed to unblock the drain mark: 'Manhole cover 22'.	Noted and condition attached
	Education	The proposed works support the agreed delivery of a reinstatement of Bounds Green Infant and Junior School	Noted.

No.	Stakeholder	Question/Comment	Response
		<p>from two to three form entry and support Education Service's plan to ensure continued school place sufficiency. In this respect we support the work proposed to achieve this.</p>	
	<p>Environmental Health Pollution</p>	<p>Control of Construction Dust:</p> <p>No works shall be carried out on the site until a detailed report, including Risk Assessment, detailing management of demolition and construction dust has been submitted and approved by the LPA. This shall be with reference to the GLA's SPG "The Control of Dust and Emissions During Construction and Demolition". In addition either the site or the Demolition Company must be registered with the Considerate Constructors Scheme. Proof of registration must be sent to the LPA prior to any works being carried out on the site.</p> <p>Combustion and Energy Plant:</p> <p>Prior to installation details of the gas boilers to be provided for space heating and domestic hot water should be forwarded to the Local Planning Authority. The boilers to be provided for space heating and domestic hot water shall have dry NOx emissions not exceeding 40 mg/kWh (0%).</p> <p>Reason: As required by The London Plan Policy 7.14.</p> <p>As an informative:</p> <p>Prior to demolition of existing buildings, an asbestos</p>	<p>Noted and conditions and informative attached.</p>

No.	Stakeholder	Question/Comment	Response
		survey should be carried out to identify the location and type of asbestos containing materials. Any asbestos containing materials must be removed and disposed of in accordance with the correct procedure prior to any demolition or construction works carried out.	

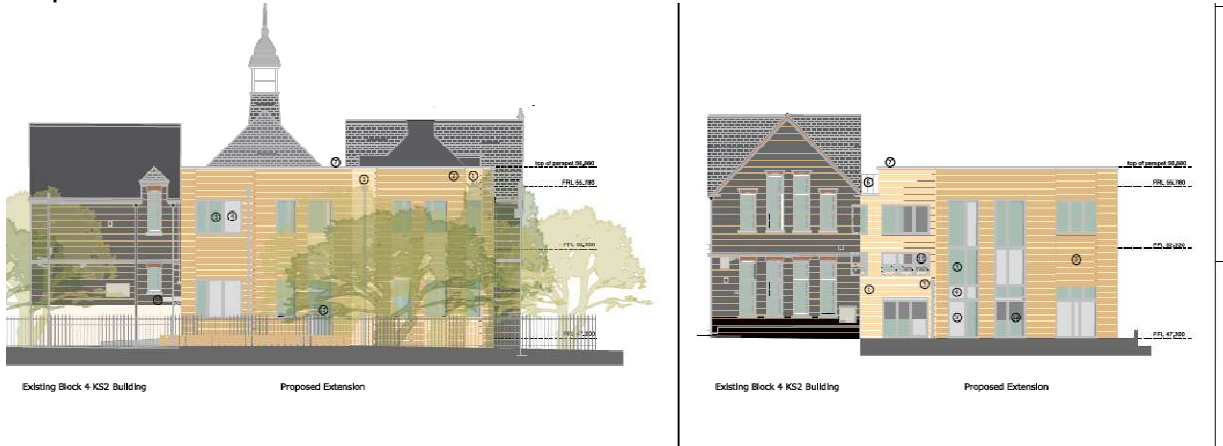
Existing elevation



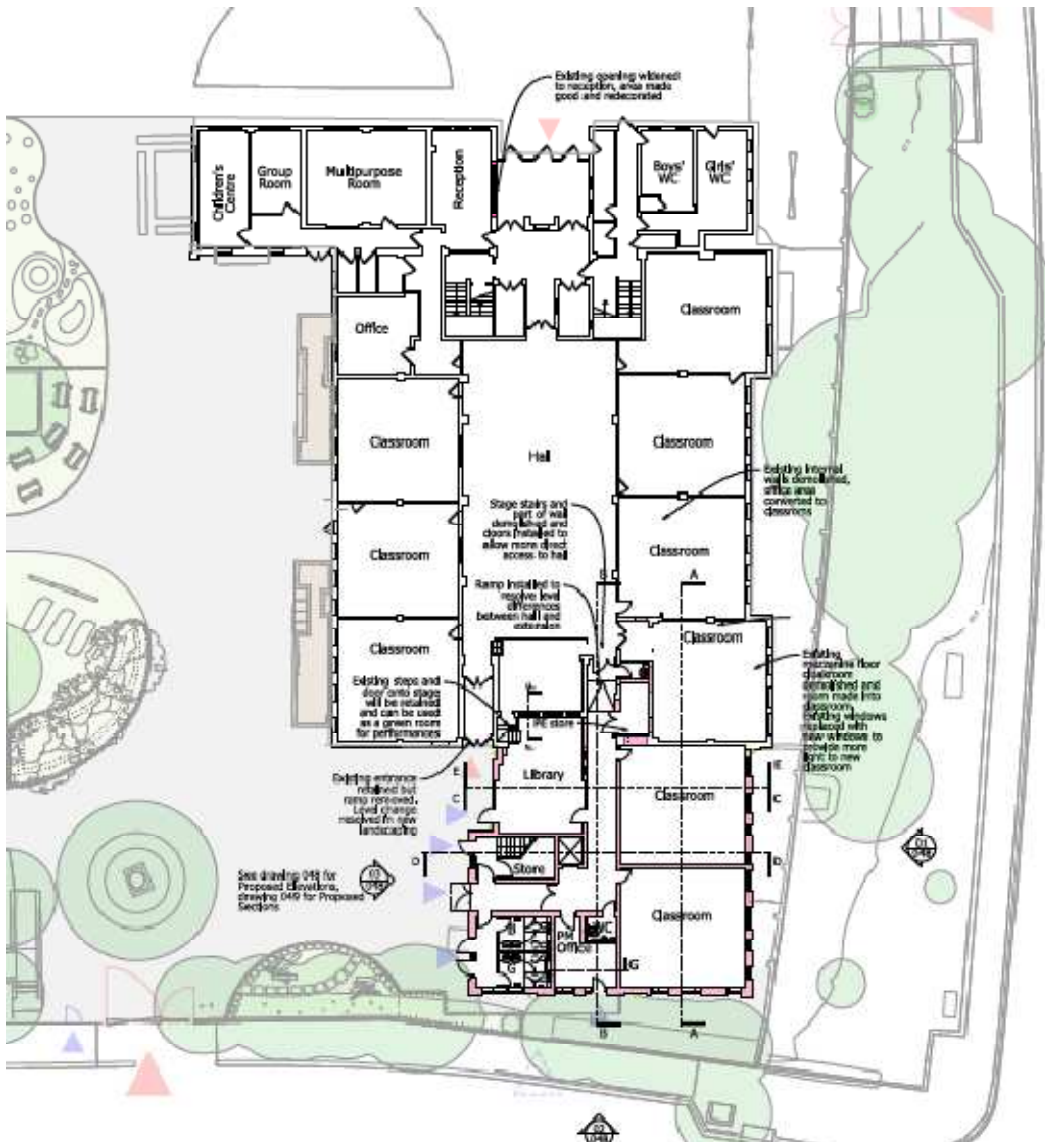
Proposed elevation onto Bounds Green Road



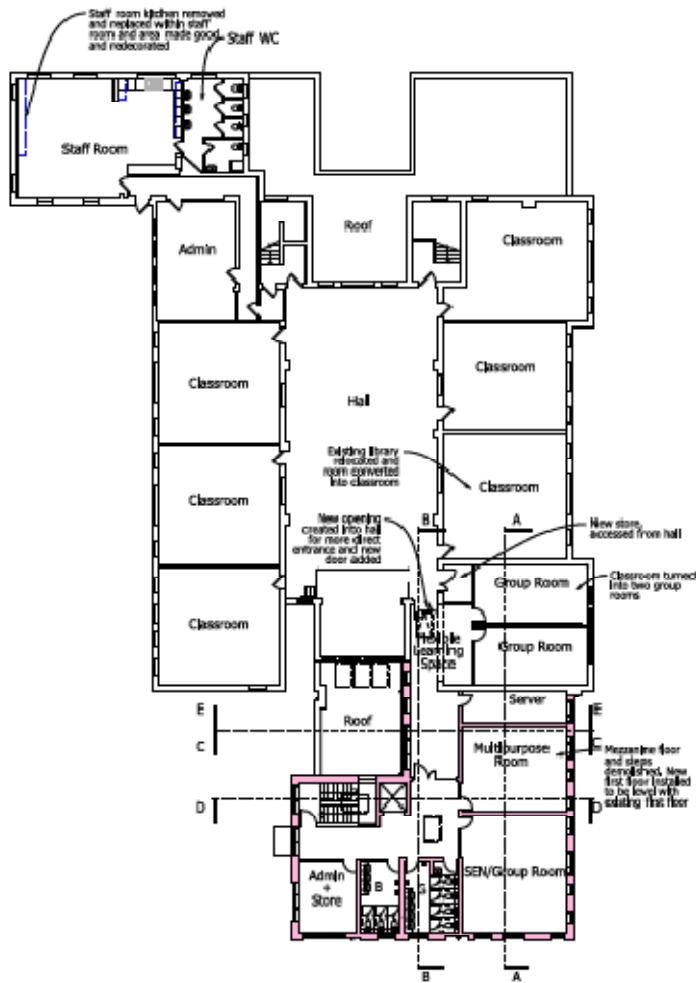
Proposed rear and side elevations



Proposed ground floor plan



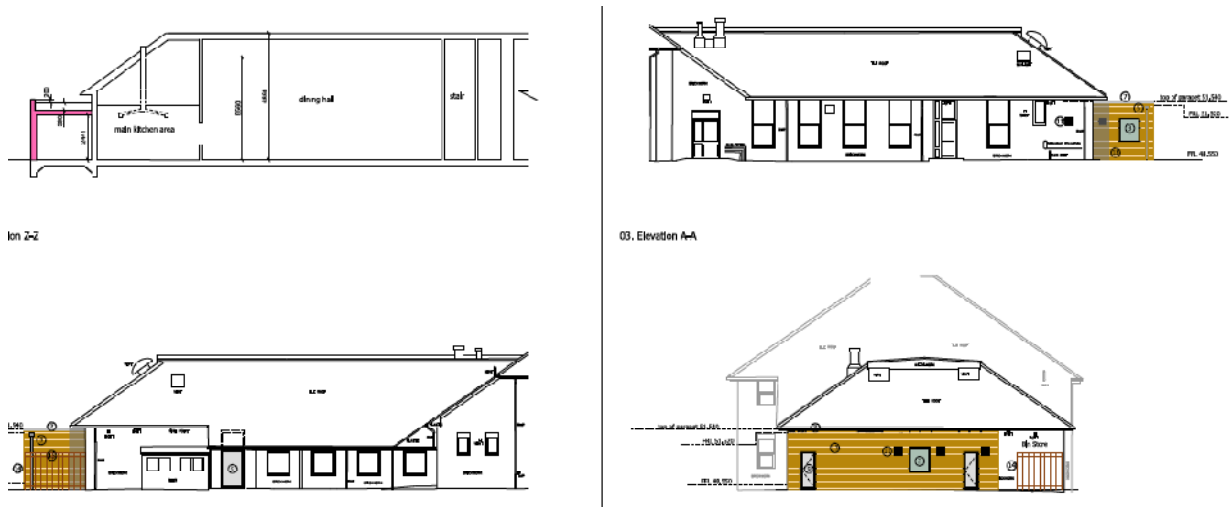
Proposed 1st floor plan



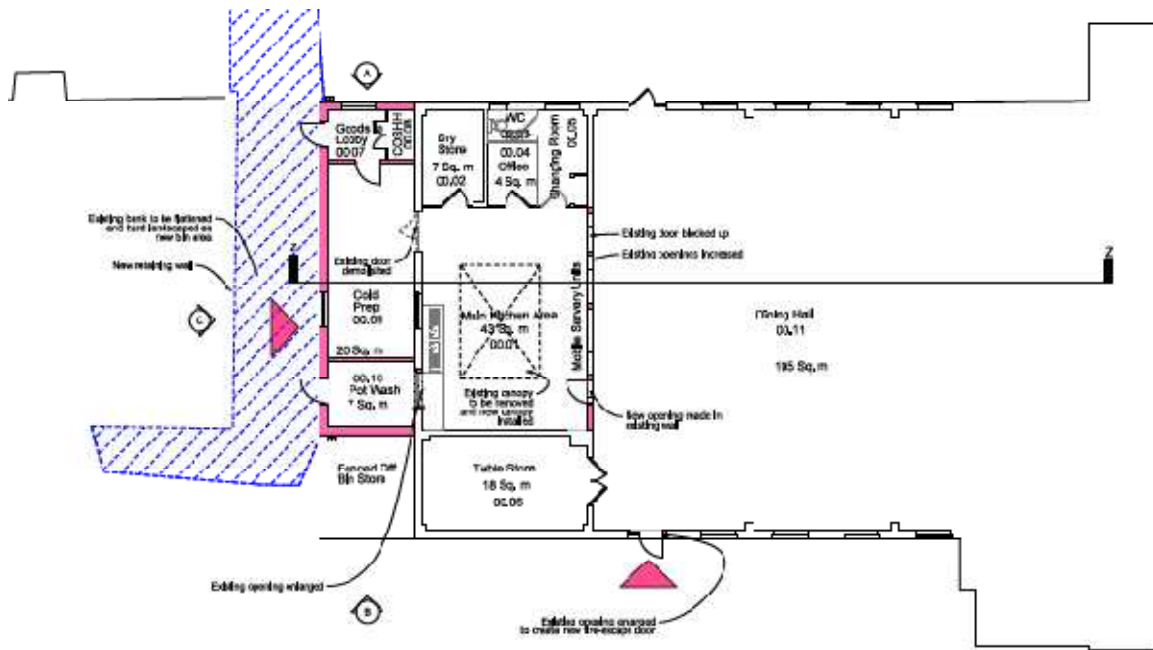
Existing kitchen



Proposed kitchen extension elevations



Proposed kitchen extension floor plan



Pre-application briefing to Planning Committee 23 July 2015**1. DETAILS OF THE DEVELOPMENT**

Ref: PPA/2015/0010

Site Address: Tottenham Hotspur Stadium, Bill Nicholson Way, 748 High Road N17 0AP

Ward: Northumberland Park

Description of Development:

Replacement 61,000 seat stadium with a retractable pitch, new club superstore and museum, 180 bed hotel, an extreme sports centre, a community medical centre, new public square and 579 residential units arranged in 4 towers ranging in height from 16 to 32 storeys located above a 2-3 storey podium. The proposals also include works to the Grade II Listed Warmington House and the demolition of three locally listed buildings.

Applicant: Tottenham Hotspur Property Company Ltd

Agent: DP9

Ownership: Private

Case Officer: Neil McClellan

2. BACKGROUND

- 2.1 Tottenham Hotspur Football Club were granted planning permission by Haringey Council in 2011 for the redevelopment of White Hart Lane Stadium and the land around it. This scheme was known as the Northumberland Park Development (NDP) scheme. Phase 1 of the NDP scheme has been completed and comprises the new Sainsbury's superstore on Northumberland Park Avenue and Lilywhite House which is home to Tottenham University Technical College and the football club's offices.
- 2.2 Earlier this year Haringey Planning Committee approved a material amendment to the approved stadium introducing a basement level to the scheme. Work has commenced on the stadium phase. Most of the land between Paxford Road and Lilywhite House has been cleared and ground works have commenced.
- 2.3 Tottenham Hotspur are now in discussions with the Council over plans to revise their proposals for the rest of the site. These changes comprise a larger stadium – 61,000 seats rather than the 56,250 approved in 2011 and more flats – 579 rather than the 285 previously approved. Other features of the revised proposal are a new club store and museum described as 'the Tottenham Experience', an extreme sports centre, a 180 bed hotel, a community medical centre and new public square. The proposals also include works to the Grade II Listed Warmington House and the demolition of three locally listed buildings.

2.4 Tottenham Hotspur has recently announced an agreement with the NFL to play a minimum of two games per year during a 10-year partnership at the proposed new stadium. As well as NFL games the Club also aspire to host other non-football events such as music concerts that the proposed new stadium would specifically designed to allow. A retractable 4G pitch has been included in the proposal for NFL games.

2.5 At a strategic level Council policy promotes regeneration in North Tottenham. The emerging Tottenham Area Action Plan supports the comprehensive regeneration of Northumberland Park including taller and higher density development near to the redeveloped Tottenham Hotspur FC Stadium.

2.6 The redevelopment of the stadium is also supported by the Council's High Road West regeneration scheme - a master planned comprehensive development creating a residential led mixed use neighbourhood opposite the stadium. It includes a new public square linking White Hart Lane Station with the redeveloped football stadium and an expanded local shopping centre on the High Road.

3. SITE AND SURROUNDS

3.1 The application site consists of approximately 11.5ha of land located around White Hart Lane Stadium. It includes most of the land between Northumberland Park, Park Lane, Tottenham High Road and Worcester Avenue.

3.2 The northern end of the site is now occupied by the completed Phase 1 'Northern Development' of the original planning permission comprising a Sainsbury's superstore and Lilywhite House which is home to Tottenham University Technical College and the football club's offices.

3.3 A large section of the central portion of the site has been cleared of buildings and ground works in relation to Phase 2 'Stadium Development' of the original permission are currently underway.

3.4 The rest of the site is occupied by the existing stadium, 3 locally listed buildings and the Grade II listed Warmington House, all located at the site's southern end facing the High Road, and the remaining Archway Sheet Metal Works Limited on Paxton Road.

4. PROPOSED DEVELOPMENT

4.1 Replacement 61,000 seat stadium, new club superstore and museum, 180 bed hotel, an extreme sports centre, a community medical centre, new public square and 579 residential units arranged in 4 towers ranging in height from 16 to 32 storeys located above a 5 storey podium. The proposals also include works to the Grade II Listed Warmington House and the demolition of three locally listed buildings.

5. PLANNING HISTORY

5.1 The Stadium and associated land have an extensive planning history with a large number of applications. The most relevant to the current application are:

HGY/2010/1000 Demolition and comprehensive redevelopment of a stadium (Class D2) with hotel (Class C1), retail (Class A1 and/or A2 and/or A3 and/or A4 and/or A5), museum (Class D1) offices (Class B1) and housing (Class C3); together with associated facilities including the construction of new and altered roads, footways, public and private open spaces; landscaping and related works. Details of "appearance" and "scale" are reserved in relation to the proposed residential and hotel buildings. **Decided 20.09.2011.**

HGY/2010/1001 Conservation Area Consent for demolition of 734-740, 742, 744a, 752a, 752b, 752c, 754-766, 768-772, 776 and 778-788, 806a, 806b High Road, N17, Paxton Hall, Paxton Road, N17, 2-6 Northumberland Park, N17 and any other buildings and structures within the curtilage of these buildings on land bordered by Northumberland Park N17 to the North, High Road N17 to the West, Park Lane N17 to the South and Worcester Avenue N17 to the East within the North Tottenham Conservation Area in conjunction with the comprehensive redevelopment of adjoining land for a stadium with hotel, retail, museum, offices and housing, together with associated facilities including the construction of new and altered roads, footways, public and private open spaces, landscaping and related works. **Decided 20.09.2011.**

HGY/2011/2350 Planning Permission for proposed demolition of buildings and development of a foodstore (Use Class A1) together with educational uses (Use Class D1); stadium-related uses (Use Class D2); showroom/brand centre (sui generis); and associated facilities including car parking, the construction of new and altered vehicle and pedestrian accesses, private open spaces, landscaping and related works. **Decided 29.03.2012.**

HGY/2011/2351 Outline Planning Permission for Proposed demolition and redevelopment to provide housing (Use Class C3) college (Use Class D1) and/or health centre (Use Class D1) and/or health club (Use Class D2) together with associated private and public open space, car parking, landscaping and related works; and altered footways, roads and vehicular accesses. Outline application with details of appearance, scale and landscaping reserved for subsequent approval. **Decided 29.03.2012.**

HGY/2012/0096 Non- material amendment following a grant of planning permission HGY/2010/1000. **Decided 28.02.2012.**

HGY/2013/1973 Variation of condition 42 attached to planning permission HGY/2011/2350 "Proposed demolition of buildings and development of a foodstore (Use Class A1) together with educational uses (Use Class D1); stadium-related uses (Use Class D2); showroom/brand centre (sui generis); and associated facilities including car parking, the construction of new and

altered vehicle and pedestrian accesses, private open spaces, landscaping and related works", for variation of fourth floor plan to allow for proposed change of use from stadium-related uses to Use Class B1a and associated minor alterations. **Decided 31.03.2014.**

HGY/2013/1976 Variation of condition 42 attached to planning permission HGY/2011/2350 "Proposed demolition of buildings and development of a foodstore (Use Class A1) together with educational uses (Use Class D1); stadium-related uses (Use Class D2); showroom/brand centre (sui generis); and associated facilities including car parking, the construction of new and altered vehicle and pedestrian accesses, private open spaces, landscaping and related works", for variation of second floor plan to allow for proposed change of use from stadium-related uses (Use class D2); showroom/brand centre (sui generis), to Class D1 to form a new university technical college and associated minor alterations. **Decided 31.03.2014.**

HGY/2014/2326 Non-material amendment following a grant of planning permission HGY/2013/1976 for variation of condition 3 in relation to rear boundary works. **Decided 26.08.2014.**

HGY/2014/2327 Non-material amendment following a grant of planning permission HGY/2013/1973 for variation of condition 3 in relation to rear boundary works. **Decided 26.08.2014.**

HGY/2015/0797 Application for approval of reserved matters relating to the scale of buildings in Phase 3 'the Southern Development' granted outline permission (HGY/2011/2351) for the redevelopment of site to provide housing (Use Class C3), college (Use Class D1) and/or health centre (Use Class D1) and/or health club (Use Class D2) together with associated private and public open space, car parking, landscaping and related works. **This application is pending.**

HGY/2015/0964 Minor material amendment to the original planning permission (HGY/2010/1000) comprising a new basement under the proposed north, east and west stands and internal modifications to the ground floor of the approved stadium. **Decided 22.06.2015.**

In addition to the above all pre-commencement conditions relating to the Phase 1 'Northern Development' and Phase 2 'Stadium Development' have been discharged.

6 CONSULTATION

6.1 Quality Review Panel

6.2 The new proposal will be presented to the Haringey QRP on Wednesday 15 July 2015. The Panel's comments will be reported to the committee in the form of an addendum.

6.3 DM Forum

- 6.4 The proposal will be presented to a Development Management Forum on the 15 July 2015. The matters raised at the Forum will be reported to the committee in the form of an addendum.

7 MATERIAL PLANNING CONSIDERATIONS

- 7.1 The main planning issues raised by the proposed development, with internal officer comment thereon, are:

1. Principle of the development – The site is located within the Northumberland Park Area of Change as set out in Haringey's Local Plan Strategic Policies. Policy SP1 promotes development in the Area of Change.

The Council's aspirations for Northumberland Park include the provision of a mix of land uses including the redevelopment of the football stadium; renewal of existing and the provision of new housing and the provision of new retail and leisure uses. New development should contribute to the provision of open space, community facilities, regeneration initiatives and employment and training schemes. New development should be high quality, sustainable, should respect its surroundings and should preserve and enhance the area's historic environment.

The Council's strategic policies (SP1: Managing Growth, SP2: Housing) seek to maximise the supply of additional housing to meet and exceed 8,200 homes from 2011-2021 (820 per annum). Consultation on the Alterations to the Local Plan Strategic Policies reflects the increase in this target within the London Plan to 15,020 homes from 2011-2021 (1,502 per annum) but retains the spatial vision for the Borough, including a focus on certain key growth areas, including Northumberland Park.

The stadium and land to the north and south of it has a site allocation (NT5) in the "preferred option" draft of the Tottenham Area Action Plan. The allocation is for the redevelopment of the existing football stadium and the introduction of residential, commercial, education, community, leisure and hotel uses, and improved public realm across the site. Specifically the allocation seeks a high quality design, that responds positively to the Conservation Area and provides an active frontage to the High Road and Park Lane; an improved interface with sites to the east of the stadium to support regeneration objectives for this area; high quality publicly accessible spaces that complement other public realm/spaces in the area; complimentary leisure/ commercial uses; and an increase in the amount of residential development on the site.

Other allocations in the draft AAP are supportive of the redevelopment of the stadium, encouraging the regeneration of the wider North Tottenham area and new development that complements a redeveloped stadium.

Draft allocations for Northumberland Park North (NT1) to the north of the site and Northumberland Park (NT2) to the east propose the masterplanned regeneration of the area to improve existing, and create new, residential neighbourhoods. The allocation identifies land adjacent to the stadium development as being suitable for tall buildings. Between them the draft allocations for the Northumberland Park Regeneration Area propose providing at least 2200 new additional homes.

The redevelopment of the stadium is also envisaged by the Council's High Road West regeneration scheme - a masterplanned comprehensive development creating a residential led mixed use neighbourhood opposite the stadium.

Given the Council's aspirations for the site and for the regeneration of the wider North Tottenham area, the principle of a bigger capacity, more versatile stadium, with an increased leisure /commercial offer, additional housing, community uses and a new public square is not inconsistent with the emerging planning policy framework. However the environmental and transport impacts of the proposal will need to be assessed and found acceptable. Furthermore any benefits arising from the proposal will need to be balanced against the heritage impacts of the proposed removal of the three locally listed buildings, the works to Warmington House and the impact of a denser, taller development on the character of the Tottenham High Road conservation area.

2. Design and appearance – As has already been stated the site has been identified in the Council's draft Tottenham AAP as being a suitable location for a taller development, however in line with the Council's emerging policy on tall buildings, policy DM5 of the Haringey Development Management Policies (Preferred Options) Document, the proposed development will have to be of the highest quality.

The applicant argues that the he proposed changes to the scheme are positive. They set out that the proposed new stadium has a much better relationship to the High Road than the consented scheme, introducing more activity and interest along it's frontage to the High Road; The creation of a new terrace fronting the High Road and housing a new club superstore and museum helps the scheme to present a continuous series of active uses and more generous public realm along the sites frontage. This new terrace will incorporate the Grade II Listed Warmington House. More detail will need to be provided to allow officers to assess the impact of this on the Listed Building and its setting. The stated improvements in the design of the new stadium and its supporting development will need to be balanced against the impact of the proposals on the heritage assets in line with the statutory test and NPPF obligations.

3. Heritage Impact– The western edge of the site lies within the North Tottenham High Road Conservation Area, which forms part of the Tottenham High Road Historic Corridor of six connected Conservation Areas. The Council has a duty under s72 of the Act to consider any

potential harm caused by the development, giving it significant weight, and considering the desirability of preserving or enhancing the character or appearance of the conservation area.

The proposal to retain and intergrate Warmington House into the new Tottenham Experience building is positive. However the justification for and assessment of the impact of the development on the heritage assets within the locality will require comprehensive assessment and justification. This will also include the need to understand and consider the impact of the taller buildings on the conservation area.

4. Transport Impact –There is already a consented scheme for a 56,250 seat stadium and the transport impacts were mitigated by a travel plan and management plans. No information on these matters has been provided to date. Work needs to be done demonstrating how the proposed increase in capacity to 61,000 spectators and the aspiration to host more non-football events at the new stadium can be accommodated by the transport network. The Council has not yet had sight of a Transport Assessment, Stadium travel Plan, Event Day Management Plan or a Stadium Servicing Plan.

There is an ongoing dialogue in relation to parking, servicing, or travel planning in relation to the rest of the scheme. However the transport impacts of this part of the scheme and in particular the additional housing will need to be fully assessed and found acceptable. Any transport assessment will need to be tested against the significant growth planned for the wider area.

The Site has a high Public Transport Accessibility Level ("PTAL") rating of 4.

The following Saved UDP and Local Plan Policies are relevant:

- 1) Saved UDP Policy M9 Car free developments.
- 2) Saved UDP Policy M10 Parking requirements for New Developments.
- 3) Local Plan Strategic Police SP1 (Managing Growth), SP4 (working Towards a Low carbon) and SP7 transport.

The proposed development will have to be supported by a transport assessment (TA); the applicant is required to conduct a joint TA scoping meeting with TfL and Haringey Transportation and Highways.

5. Environmental Impact – A draft scoping opinion has been circulated and officers are currently forming a response. Like most of London all of Haringey exceeds national emission targets. Given Haringey's responsibility under Local Air Quality Management (LAQM) legislation the Council is required to put measures in place to reduce emissions. The impact on air quality will need to be considered and given the aspirations to hold music concerts and other public events in the evening, the impact of noise from these and other

events will need to be assessed.

6. Public Realm – As well as the more generous public realm along the High Road frontage discussed above the new scheme also proposes the creation of a new public square on the podium to the south of the stadium which will be comparable in size to Trafalgar Square. Further development work is expected on the public realm around the site including the square and the shared amenity space serving the new homes.
7. Affordable housing – The scheme currently proposes 579 residential units. Initial indications are that the development will contribute towards the Council aspiration for new affordable homes. Discussions on the precise format and quantum are ongoing.

The preferred affordable housing mix, in terms of unit size and type of dwellings on individual schemes will be determined through negotiation, scheme viability assessments and driven by up to date assessments of local housing need and will be secured via a section 106 agreement.

8. Impact on residential amenity – Any design proposal should consider the impact on the amenity of the surrounding properties. . A daylight & sunlight and shadowing report would be expected with the submission of a planning application taking into account the proposed units and any sensitive surrounding uses.
9. Quality of accommodation – All accommodation must accord with Haringey and London Plan standards. Currently, there are no floor plans of the proposed units and therefore this aspect cannot be formally commented on. The development will also need to consider air quality conditions in the locality as part of the design process.
10. Sustainability - The site has been identified as being in an area with potential for being part of a decentralised energy network. This may be as a decentralised energy hub, as a customer, or requiring part of the site to provide an easement for the network.

The applicant is preparing a comprehensive statement to accompany the proposal exploring the feasibility of all potential sustainability measures.

Aerial View from South



Aerial View from West



High Road View from South



Dial House View



